

Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods

Washington State Department of Transportation Paul Kinderman AIA Sandy Salisbury RLA Darrel McCallum PE Eastern Region Project Engineer 11 2015

Neighborhood Locale Design Elements

Trent Avenue Industrial Area



NSC travelers Bridge over Trent Industrial Area

NSC travelers ramp bridges over industrial area

Bicycle Pedestrian Trail

Trent Avenue Pedestrian Bridge

Railroad Pedestrian Bridge

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Trent Industrial Zone

3D Computer Study Model

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Idea Pad

Notes

- ① USE LIGHTING FOR VISUAL INTEREST. CONSIDER L.E.D. OR SOLAR TO REINFORCE 'HIGH TECH' IMAGE. THE BRIDGE SHOULD EXPRESS THE NOBILITY OF HUMAN INDUSTRIAL PRODUCTION.
- ② POSSIBLE GATEWAY LOCATIONS EXIST WITH OPPORTUNITIES FOR SPECIAL LANDSCAPE ARCHITECTURE DESIGNS.

Handwritten Notes:

- ① LOOK FOR BRIDGE DESIGNS THAT EXPRESS THE EMERGING 'HIGH-TECH' ENTERPRISES OF THE INDUSTRIAL ZONE.
- ② LANDSCAPE OPPORTUNITIES APPEAR AT RAMP TOUCH DOWN AREAS.
- ③ BE AWARE OF AND DESIGN FOR LONG VIEW SHEDS AT FREYA CROSSING?

Map Labels: E. Shorge Avenue, E. Borne Avenue, East Trent Avenue, Children of the Sun Trail, North Spokane Corridor, Fairview Avenue, Trent Industrial Zone.

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Idea Pad

Notes

- ① EXPRESS THE NATURE OF THE NEIGHBORING INDUSTRIAL ZONE IN THE PEDESTRIAN BRIDGE.
- ② VIEWS OF THE BRIDGE FROM FREYA WAY & FROM EAST TRENT AVE NEED TO REINFORCE THE POSITIVE ASPECTS OF THE INDUSTRIAL ZONE.

Handwritten Notes:

- ① PEDESTRIAN BRIDGE WITH LOCAL SPECIFIC THEME.
- ② IMPORTANT VIEW SHEDS AT FREYA WAY & AT TRENT AVENUE.

Map Labels: E. Shorge Avenue, E. Borne Avenue, East Trent Avenue, Children of the Sun Trail, North Spokane Corridor, Fairview Avenue, Trent Industrial Zone.

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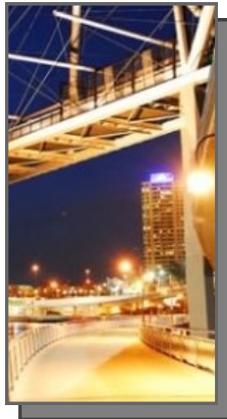
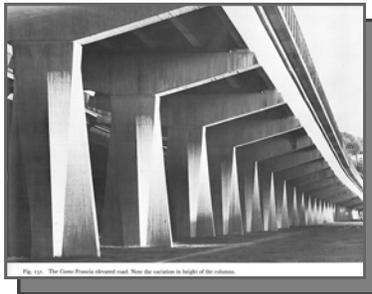
Post Workshop I Design Team Summaries

Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods

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Trent Avenue Industrial Area

Recommendations



Express the industrial nature of the setting in the bridge designs.

The aesthetics should celebrate the nobility of human technical accomplishments. Use LED or solar lighting to accent the area.



The pedestrian bridge should be memorable. It should be designed to represent the positive aspects of an industrial zone.

AESTHETIC GOALS

Create an open and well lit space under the bridge.

AESTHETIC CHARACTER

- Modernistic
- Expressive of the nobility of technology and Industry

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Trent Avenue Industrial Area

Recommendations



Design with pine trees for a unified corridor look.



Plant native grasses for its drought tolerant characteristics.



Use lilacs in prescribed areas to represent Spokane as the Lilac City.



Gateway elements and neighborhood entry way focal points should be provided at the ends of approach ramps.

AESTHETIC GOALS

Create a focal point for NSC users entering the Industrial area via the ramps.

AESTHETIC CHARACTER

- Naturalistic
- Seasonally colorful focal points

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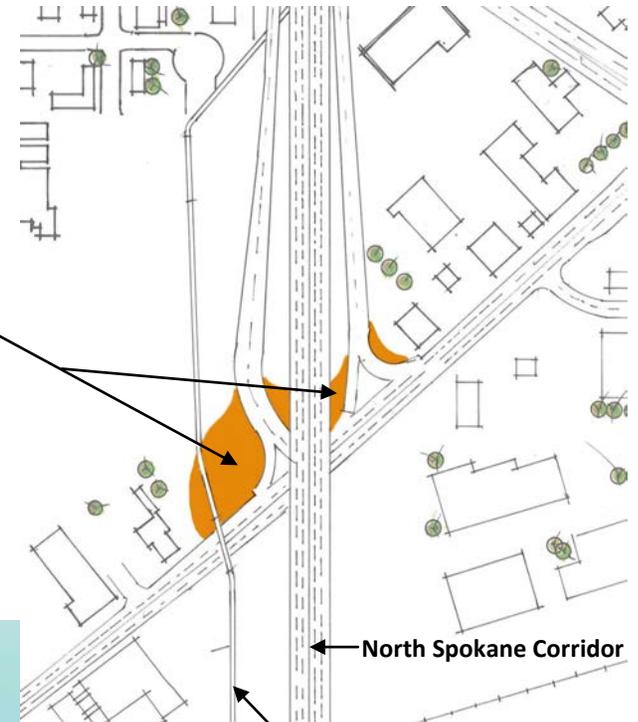
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Trent Avenue Industrial Area

Landscape Study

Landscape composition will transition to primarily deciduous trees, native grasses and forbs. Additional visual cues to this industrial gateway may include basalt rocks for seating especially near the bicycle pedestrian trail.

Landscape opportunities at ramp touchdown areas



North Spokane Corridor

Children of the Sun Trail



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Neighborhood Locale Design Elements East University District



Railroad Pedestrian Bridge

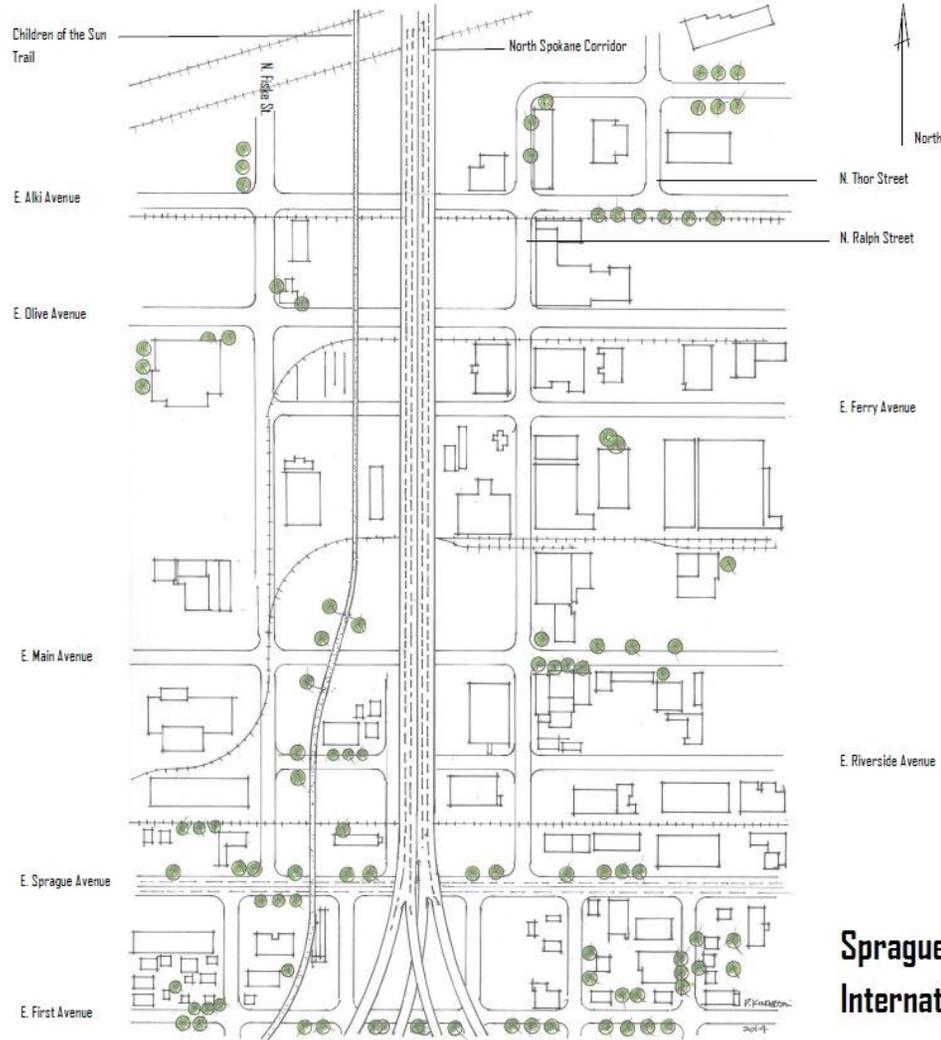
NSC travelers Bridge over Trent Industrial Area and Sprague Avenue

Bicycle Pedestrian Trail

Sprague Avenue Pedestrian Bridge

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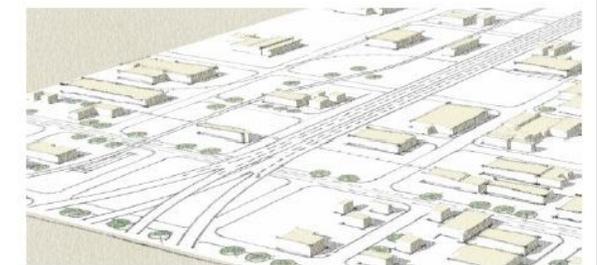
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Idea Pad

Notes

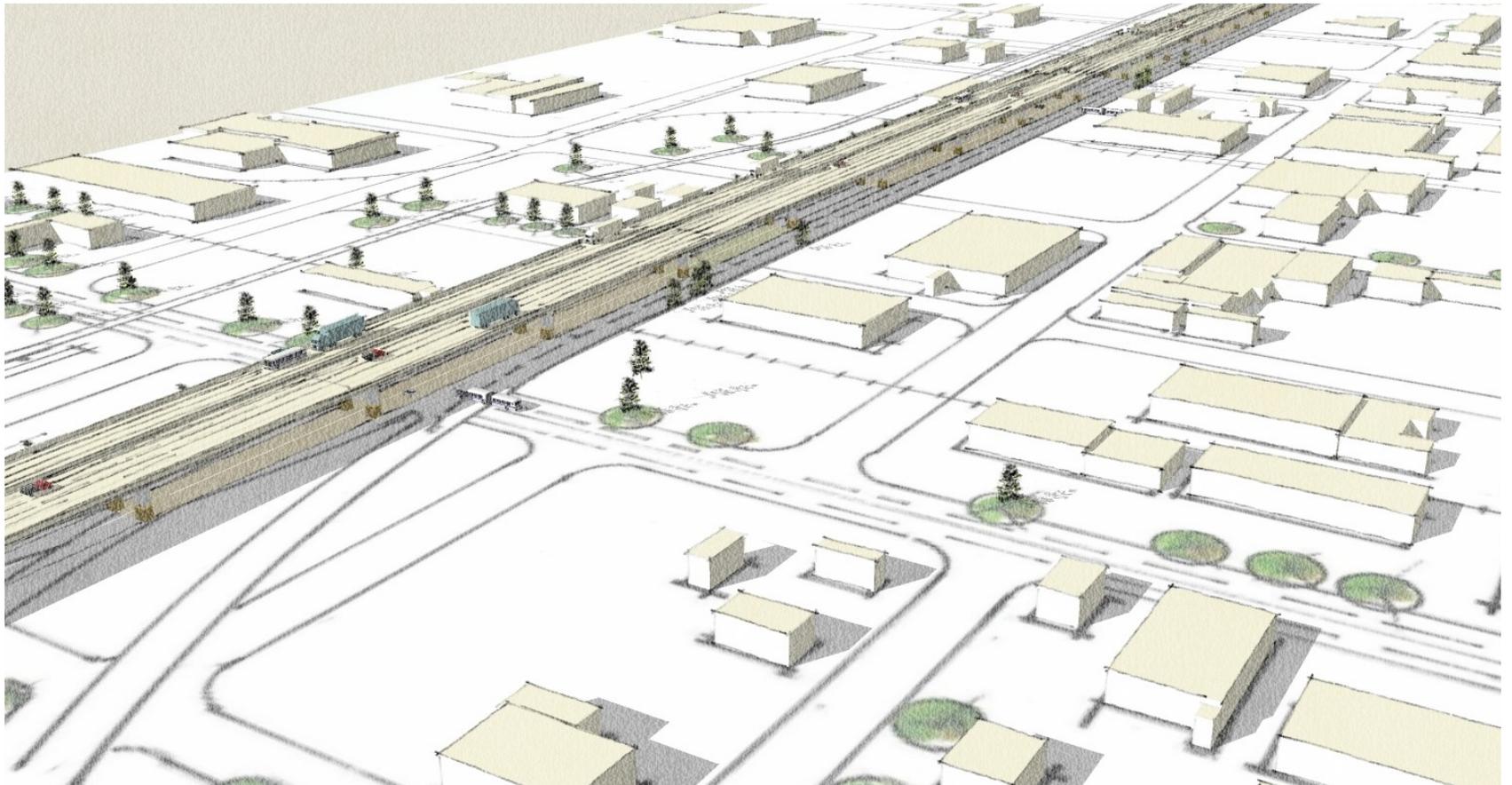
Sprague Avenue & International District



Workshop Tool

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**East University District
3D Computer Study Model**

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Idea Pad

Notes

1. THIS AREA HAS A STRONG HISTORIC COMPONENT. RECALLS THE RACE TRACK AT PLATEAU WHERE DESIGNING MOTIFS. HOWEVER, BE RESERVED IN CHOOSING 'NOSTALGIC' THEMES. LOOK FOR 'INTO THE FUTURE' DESIGNS.
2. MAKE A VERY VISUAL CONNECTION WITH THE PROPOSED UNIVERSITY PEDESTRIAN BRIDGE. USE THE SPRAGUE CROSSING AS A GATEWAY TO THE INTERNATIONAL DISTRICT.

RECALL THE HISTORY OF HORSE RACING IN MOTIFS.

THE BRIDGE AS VIEWED FROM SPRAGUE IS A BOOKEND TO THE INTERNATIONAL DISTRICT.

BOOK ENDS TO INTERNATIONAL DISTRICT

GATEWAY TO INTERNATIONAL DISTRICT

Sprague Avenue & International District

G-2244

Idea Pad

Notes

1. TREAT THE BRIDGES IN THE NORTHERN SECTOR OF THIS LOCALITY WITH SIMILAR AESTHETICS AS THE TRUST INDUSTRIAL ZONE.
 - LIGHTING FOR VISUAL INTEREST & SAFETY
 - SOLAR OR L.E.D. HIGH TECH
 - STRONG SCULPTURAL SUBSTRUCTURE ELEMENTS
2. VIVID GOOD LOOKING PEDESTRIAN CROSSINGS

THE BRIDGES SOUTH OF RAY YARD NEED TO BE OF THE SAME VISUAL FAMILY AS THOSE NORTH IN THE TRUST INDUSTRIAL AREA

GENERAL CONSENSUS THAT EARLY NOISE WALL STUDIES BY NEIGHBORHOOD THEME ARE STILL VALID IN THEIR PARTICULAR AREA. HOWEVER DO NOT USE A BRAND BASED "NOT TO THE PAST" AESTHETIC PHILOSOPHY.

Sprague Avenue & International District

G-2244

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East University District

Recommendations

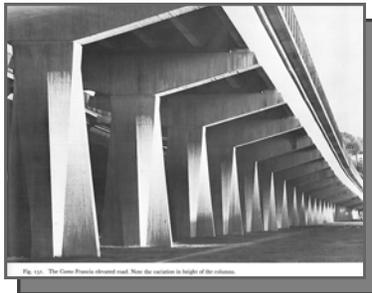
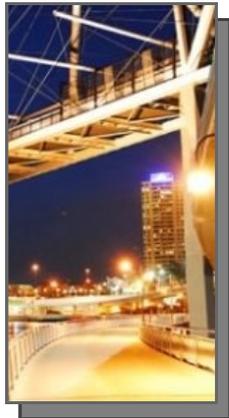


Fig. 122 The Great Pacific elevated road. Note the variation in height of the columns.



Express the high technology nature of the setting in the bridge designs. Visually tie the Sprague Avenue bridges to the Trent Industrial zone.



Like the Trent Industrial zone bridges, the aesthetics should celebrate the nobility of human technical accomplishments. Use efficient lighting to accentuate the bridges.



The bridge crossing at Sprague Avenue should act as 'book ends' to the new Intercollegiate center bridge.



AESTHETIC GOALS

Create a strong visual east west connection to Sprague Avenue travelers.

AESTHETIC CHARACTER

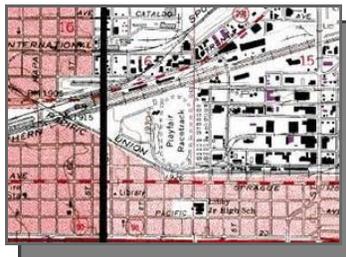
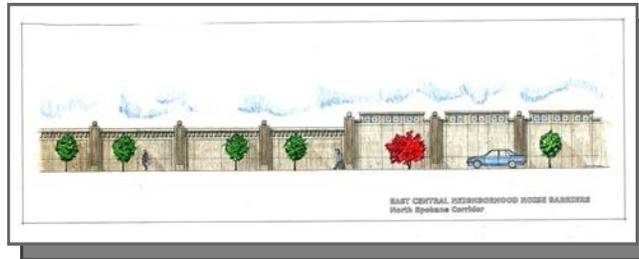
- Modernistic
- Expressive of multi cultural enterprise

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East University District

Recommendations



For design motifs, recall the history of the area. Playfair racetrack and it's horse racing history should be referenced.



The pedestrian bridge should be memorable and designed to positively represent the complex multicultural nature of the Sprague area.

AESTHETIC GOALS

Express local history and culture through design motifs.

AESTHETIC CHARACTER

- Modernistic
- Expressive of multi cultural nature of the East University District.

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East University District

Recommendations



Pines should be used throughout the corridor to harmonize with Spokane area flora.



Plant with native grasses for conservation of water.



Use lilacs for accents in the 'Lilac City.'

AESTHETIC GOALS

Create a unified landscape design with interesting focal points.

AESTHETIC CHARACTER

- Naturalistic
- Seasonally colorful focal points

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East University District



Columns with high tech look and feel.

Unique traffic barrier to be determined.

Elevation View looking east from Sprague Avenue

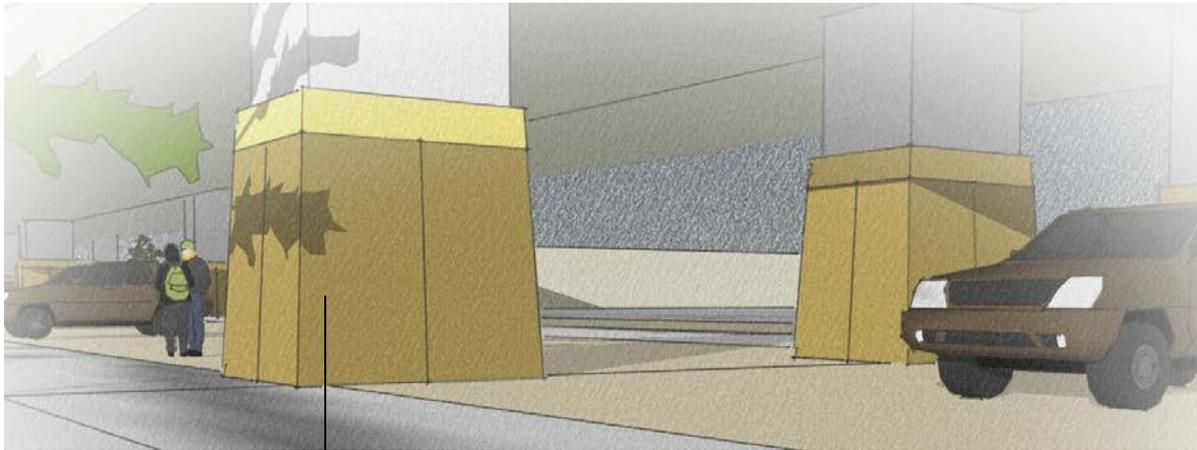
Form Study

Columns under bridge soffit.

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East University District



All of the columns have heavy strong bases. These provide a visual anchor.

The bases are similar to those in the Skyway. However they have a high tech industrial look and feel. They are distinctly modern.

Landscaping will be determined as the design proceeds.

Elevation View looking north from Sprague Avenue

Form Study

Columns under bridge soffit.

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East University District



Separate and unique pedestrian bridge.



Discussion:

The pedestrian bridge may be separated from the mainline bridge. It then becomes a natural candidate for a gateway to the International District.

By creating a character sympathetic to the proposed Intercollegiate center bridge, it can become metaphorical 'book ends' to the district.

Form Study

The separate pedestrian bridge.

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East University District



Recent work by Sound Transit in Seattle's SODO district relies on referencing contextual elements.

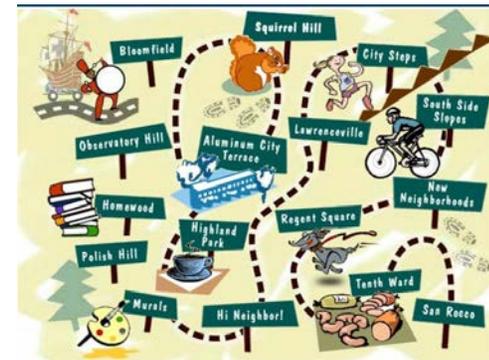


Consider faux structural features evocative of structural art.

Low cost non load bearing cable elements can create imagery sympathetic to Spokane's Intercollegiate Center bridge.

The design team should be aware that the proposed city infrastructure will become part of the physical context by the time the NSC bridges are built.

The design team should reference the "It's the Neighborhood" series in Pittsburg for Context Sensitive Design discussions in city urban design discussions.



"It's the Neighborhood" series by WQED Pittsburg.

Character Study

Create 'bookends' for the International District.

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NORTH SPOKANE CORRIDOR DESIGN ADVISORY GROUP WORKBOOK

Appendix

- Meeting 3: Evening Workshops Design Team Notes
- Meeting 4: Evening Workshops Design Team Notes
- Current North Spokane Corridor Aesthetics
- Authors



Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods

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North Spokane Corridor

Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods

Design Team Notes

Date: May 7 & 8, 2014

Meeting 3: Evening Workshops

Notes are nominally from the north and proceeding south. Submitted in bullets and prior to evaluation or ranking.

- Consider WSU student work and opportunities for integration.
- Integrate the Hillyard communities mural concepts to the extent possible in the 'mural wall'. Note that the neighborhood adopted the designs over time through it's own public process.
- Various 'mural wall' configurations explored. Integration of 'mural wall' and ramp will be investigated.
- Curtain wall basic form and bridge barrier horizontal bands adopted by consensus. The form will be altered with details and finishes customized by locale.
- Additional northern ramp for Rowan pedestrian bridge proposed for youth access from adjacent swimming pool. Explore small meeting place 'belvedere' at intersection of ramps and bridge so as to integrate natural surveillance aspects.
- Retaining wall locations and height mapped and studied with respect to views to neighborhoods.
- Curved abutment faces proposed for selected bridges north of the river, such as Euclid Avenue. Consider creating visual interest and continuity with sloped or angled abutment faces. These designs, popular in prior eras, now have a 'retro' look that could be viewed as modern.
- Further study of 'window' opening of Euclid Avenue is required per workbook page 35. There is a balance between abutment wall size with respect to aesthetic opportunities and graffiti risk. Additionally traffic calming and the sense of pedestrian safety is affected by the opening size. Geometry decisions will inform other neighborhood crossings such as Wellesley Avenue.
- Ponderosa pine trees and lilac shrubs proposed. The ponderosa is regionally significant and recently adopted as the official city tree.
- Native grasses and basalt rock landscaping are acceptable in many locations.
- Avoid placing basalt rock work adjacent to concrete form-liners using basalt.

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North Spokane Corridor

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Design Team Notes

Date: May 7 & 8, 2014

Meeting 3: Evening Workshops

- Consider transparent noise walls or portions thereof in appropriate locations.
- Pedestrian bridges should stand out in the landscape differently than roadway bridges to assure way-finding for youth and the elderly. The pedestrian bridges need not be identical to one another.
- The Wellesley roadway ramp bridges should have same superstructure type. Consider horizontally curved prestressed girders for southern of the two bridges.
- Recall local history of the presence of wild horses when contemplating naturalistic themes adjacent to Minnehaha.
- The Chief Garry mural on Skyway noise-walls recommended. Consider historic timeline theme.
- Look for ways to integrate SCC sasquatch mascot. Stylized integration in column form has opportunities similar to Francis Avenue bridge.
- Consider skyway bridge aesthetics within the context of individual physical settings as well as a unified whole.
- Consider the pedestrian/ bicycle route integration into the skyway structure.
- Look for areas in the Chief Garry neighborhood where views on East-West streets and alleyways highlight columns. The visual quality of the columns and superstructure should be high quality to optimize these views.
- The basalt concrete finish used for walls and in the horizontal bands of bridge barriers may have use in southern parts of the corridor. It's a good look and may be applicable in certain locations.
- Provide opportunities for local art at selected locations. Wall surfaces and forms such as used on the Francis Avenue bridge serve as models for activating pedestrian spaces through neighborhood art programs.

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North Spokane Corridor

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Design Team Notes

Date: October 2 & 3, 2014

Meeting 4: Evening Workshops

Notes are nominally from the north and proceed south. They're shown in bullets and are prior to evaluation.

GARLAND LOCALE RECOMMENDATIONS.

- Consider a landform in lieu of retaining walls at the northwest corner of Wildhorse Park. This is where the pedestrian bridge 'touches down' on the east side. The landforms can serve as additional viewing areas for natural surveillance. And they provide higher visual quality than concrete walls only.
- The trail should accommodate neighborhood electric vehicles (NEV)/golf carts.
- Integrate pathway lighting at the trail and the stairway.
- The city's Wayfinding Committee is considering art deco designs, among other styles. This may integrate well with current NSC concepts.
- Consider vandalism potential and how to mitigate that.
- Consider impacts of the pedestrian crossing to the bus stop at Garland and Haven. Contacts provided by Terrell Black are Karl Otterstrom Transit Planner with STA and Dave Bentz representative to the Minnehaha neighborhood.

CORRIDOR RECOMMENDATIONS.

- The overall 'corridor recommendations' of previous workshops received broad consensus.
- Integrate the forthcoming City of Spokane Wayfinding Committee design themes. The findings will be out in approximately six months.
- Consider temporary watering for plantings.
- Reference the Chambers Bay Pedestrian Bridge in University Place Washington for an excellent example of belvedere design.
- The Skyway bridge can integrate artwork cast into the concrete barrier. This will be visible to the neighborhood. It can feature an historic theme mural and be produced in collaboration with SCC college students. Reference the 15 4th Avenue bridge project in Mt. Vernon for similar student participation.
- Remove 'Chief Garry' from the Skyway name since it result in confusion over location.

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North Spokane Corridor

Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods

Design Team Notes

Date: October 2 & 3, 2014

Meeting 4: Evening Workshops

CORRIDOR RECOMMENDATIONS

- Some college parking will be displaced as a result of the project. Consider how transit use will be affected, and the effect of less parking on transit (ridership).
- Integrate Low Impact Development design in the new parking lot design compatible with the Spokane Community College master plan.
- The Skyway structure color should provide contrast for any potential artwork.
- Spokane Community College representatives highly favored *Workbook* examples on page 89. Especially popular was the six sided plan view shape shown in the second row. This is a similar form to the NSC Wandermere bridge.
- Consider lighting the Skyway bridge both from below and above. In addition to minimum safety standards the lighting should be high-tech and modern. Consider lighting as a combination of wayfinding and traffic calming. Other current Washington state projects, such as the 520 floating bridge use this approach at deck level.
- At Trent, consider showing landscape renderings that depict a different character of seasonal interest (i.e. Winter season with the branches dominating the scene as opposed to trees in full leaf)
- When selecting plantings consider winter views and bark color or texture for winter interest.
- Consider innovative designs for noise walls that could break up the wind.
- The faux cable stay bridge approach to aesthetics is acceptable. The image at the lower left of page 108 is especially appealing in it's high tech imagery.

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North Spokane Corridor

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Design Team Notes

Date: October 2 & 3, 2014

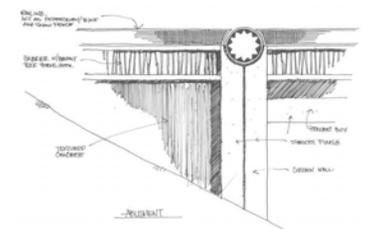
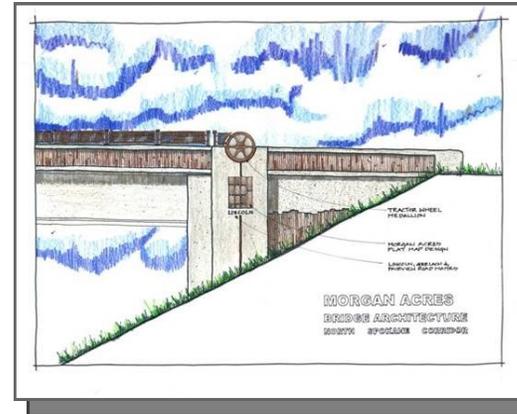
Meeting 4: Evening Workshops

CORRIDOR RECOMMENDATIONS.

- Integrate landscape planting with the noise wall designs to the extent feasible.
- Landon reminded us to be careful with the use of basalt concrete textures from Sprague Ave to Trent and I90. Avoid placing faux basalt finishes adjacent to real basalt.
- Consider that the "Chief Garry" reference to the Skyway be applied only to the Chief Garry neighborhood area.
- The separate structure for the 'shared-use-path' at the river crossing should facilitate more than just tying into the "Children of the Sun Trail". It can act as a destination itself as well as connecting to other regional trails, such as the East Upriver Drive Centennial trail.
- The large pedestals shown on the skyway seemed too big for the Sprague Avenue crossing. Look for more graceful less bulky designs.
- The NSC mainline on/off ramps for the Trent interchange are right over the Freya St. Bridge. This would lend itself to longer spans with irregular substructure geometric locations of structural elements. Investigate superstructure types, such as steel, that complement the constraints. Structure types suggested include but not limited to: weathering steel plates or boxes, PT Concrete box and segmental boxes.
- Consideration should be given to separating each direction of the mainline as well as careful location of the ramp merges. In this scenario the footprint remains the same, and only the vertical location is changed.
- Note that the Sprague Avenue & International District locale will become known as the East University District. There should be strong visual ties between the University District and the East University District.
- For the future Sprague Ave & I90 Landscape Concept: "Fix what was once lost". At the end of the corridor there are remnants of Olmstead Park Elements that were impacted from previous projects. We should make it look like a 'cared for' area and try to tie into what was once there. (Hamilton Street, Liberty Park, 3rd and Arthur St).

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Current North Spokane Corridor Bridges

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Elevation View

Sectional Elevation View

Inspirational Images

**NORTH SPOKANE CORRIDOR
FRANCIS AVENUE GATEWAY PS&E DESIGN**

Washington State Department of Transportation



Current North Spokane Corridor Bridges

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