

NORTH SPOKANE CORRIDOR DESIGN ADVISORY GROUP WORKBOOK

FINAL RECOMMENDATIONS EDITION

The North Spokane Corridor Design Advisory Group Workbook Final Recommendations Edition: Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods reports on the final recommendations of the Interdisciplinary Design Advisory Group.

The recommendations will influence the production of Architectural Standards and guide future designers as the corridor is built out.

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Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods

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INTRODUCTION

Contributing Groups

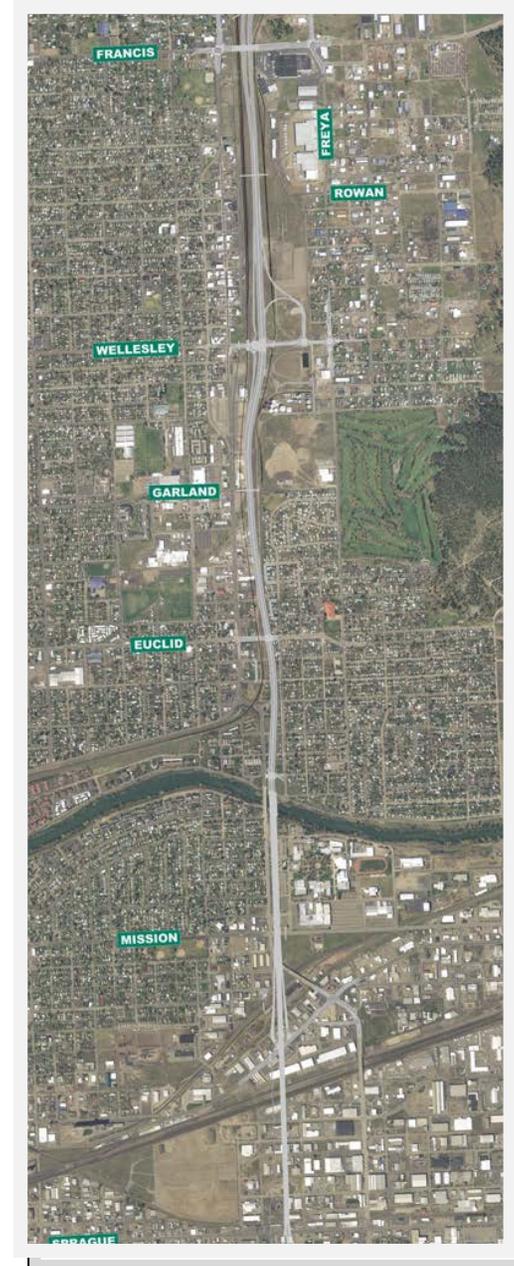
The *North Spokane Corridor Design Advisory Group Workbook Final Recommendations Editions: Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods* reports on the recommendations of the Design Advisory Group. The project phase includes the area from Francis Avenue to Sprague Avenue.

The neighborhoods are defined by the City of Spokane. The Bemiss, Hillyard, Minnehaha, Chief Garry Park and East Central Neighborhoods have representatives appointed by the Mayor. They represent organized entities within the project.

Other contributing groups included local business organizations and businesses, aspiring civic leaders, historians, city design professionals, local colleges and academics, artists, and retired academics.

The Design Advisory Group met six times between February and October of 2014. The recommendations were chronicled in a 'living workbook' format. The workbook was updated continuously and distributed through email and the WSDOT project internet web site.

This document is the final edition version of the Workbook. It shows only the final recommendations and accompanying studies.



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Context Sensitive Design & The Federal Highway Administration

The Washington State Department of Transportation implements the Federal Highway Administration's Context Sensitive Design (CSS) principles in the design of this project. CSS is a model for transportation project development that has received broad acceptance. Its essence is that a proposed transportation project must be planned not only for its physical aspects as a facility serving specific transportation objectives. It must be designed also for its effect on the aesthetic, social, economic and environmental values, needs, constraints and opportunities in a larger community setting.

WSDOT endorsed CSS for all projects, large and small, by Executive Order 1028 in 2003 under Secretary of Transportation Douglas McDonald. After that Secretary Paula Hammond continued the practice and resigned an edited order. Standing Secretary of Transportation Lynn Peterson is currently re-doubling these efforts.

WSDOT will continue to work with the neighborhood councils to incorporate mutually agreeable and feasible details into the design. The department will provide designs within the policies that guide our work and our available funding.

Practical Design

WSDOT will also employ practical design methods. The method, instigated by FHWA, delivers focused benefits for the State's transportation system working with the realities of a fiscally constrained funding environment.

The landscape design will be cost effective. In general it will consist of non-irrigated native grasses and pine trees as well as lilac bushes where appropriate. These will be combined with other aesthetic features such as columnar basalt elements. The design will meet the requirements of the Roadside Policy Manual with an emphasis on environmental stewardship through low maintenance.

Bridges, sign bridges, retaining walls and noisewalls will be designed to harmonize with existing structures. Structures that are visually part of adjacent neighborhoods will include architectural treatments. These designs will also use cost effective solutions.

Funding of features beyond cost effective designs must be provided by others through agreement.

Aesthetic Design

The goal of the recommendations is to influence the production of Architectural Standards. These will guide future designers as the corridor is built out. The recommendations include 1.) corridor wide designs and 2.) designs for specific locales.

Architectural Standards will be developed to aide in final design. They are conceptual recommendations which outline typical situations. Throughout each phase of design and construction exceptional situations may arise in which a standard solution is not applicable. Specific situations must be assessed individually so that alternatives harmonize with the project as a whole.

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NORTH SPOKANE CORRIDOR DESIGN ADVISORY GROUP WORKBOOK

PART A

The Neighborhood Locales

The neighborhood locales are defined for organization. They include major arterial intersections, the Spokane River, Spokane Community College, the emerging high tech industrial area and the East University District.

The 'locales' exist within the city's neighborhood areas. They are not determined by the individual neighborhood boundaries, but rather by their importance as nodal points in the corridor.



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Neighborhood Locale Map



Tentative Rowan Avenue Pedestrian Bridge
& Tentative Mural Wall

Wellesley Avenue

Garland Avenue Pedestrian Bridge

Euclid Avenue

The River Crossing Bridge

Skyway

Trent Avenue Industrial Zone

East University District

Eight unique localities

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Infrastructure Elements

The infrastructure elements are shown for reference.
These elements will be designed for the Architectural Standards:

- **NSC Bridges**
- **Pedestrian Bridges**
- **Noisewalls**
- **Retaining Walls**
- **Tentative Mural Wall**
- **Landscape Planting**
- **Pedestrian and Bicycle Trails**
- **Opportunities for Community based public art**
- **Sign Bridges and Luminaires**

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PART B

Corridor Wide Design

The corridor wide design is primarily concerned with NSC travelers views of the corridor. But it may include common elements, such as noise walls that are also viewed by the neighborhood.

Some recommendations are specific such as recalling local native horse populations in possible aesthetic motifs. While other recommendations invoke classic urban design safety measures, such as 'activating' public spaces with high quality 'people places'.

The recommendations are shown in bullet form to describe the *aesthetic goals*. Aesthetic goals describe the intended overall effect to be achieved.

Graphics and photographs illustrate the desired *aesthetic character*. The aesthetic character provides designers with the qualitative 'look-feel-function' of each element.



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Corridor recommendations : Design Elements for Structures

- Public art opportunities



- Activated public spaces



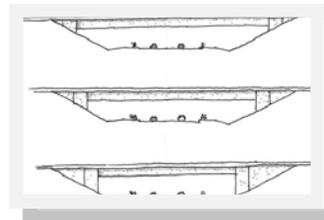
- Vivid pedestrian bridges



- Partially transparent noise-walls in selected locations. Articulate for depth using concrete masonry units.



- Balanced neighborhood street openings

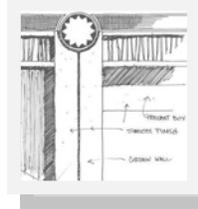


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Corridor recommendations : Design Elements for Structures

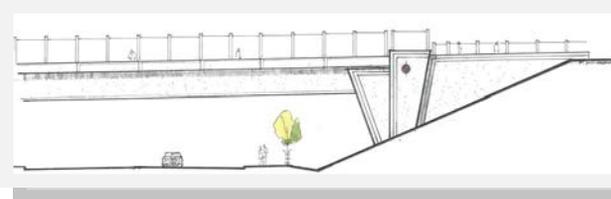
- Bridge abutment curtain walls with locale specific designs



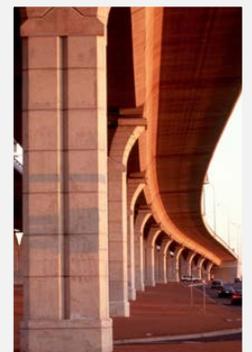
- Horizontal bands in parapet barrier with locale specific designs



- Sloping bridge abutments with subtle Art Deco styling at neighborhood side streets



- Detailed bridge columns



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Corridor recommendations: Design elements for Landscape

- Continue to use basalt wall textures among others



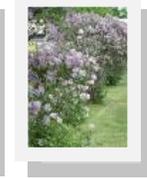
- Design columnar basalt elements



- Plant ponderosa pine trees throughout



- Use lilac bushes where appropriate



- Plant native grasses to conserve water



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PART C

Neighborhood Locale Designs

The neighborhood locale designs are concerned with views of the corridor from the neighborhoods. They respond to the unique and individual circumstances of each locale. Locale designs pay more attention to what the neighborhood sees.

For instance the Garland Avenue Pedestrian bridge connects a small neighborhood park with a well developed local shopping area. By contrast, the elevated roadway to the south is in the emerging high-tech light industrial area.

Each locale includes:

- Neighborhood Locale maps
- 3D computer study models
- Post workshop design team summaries
- Neighborhood Locale design recommendations
- Design studies



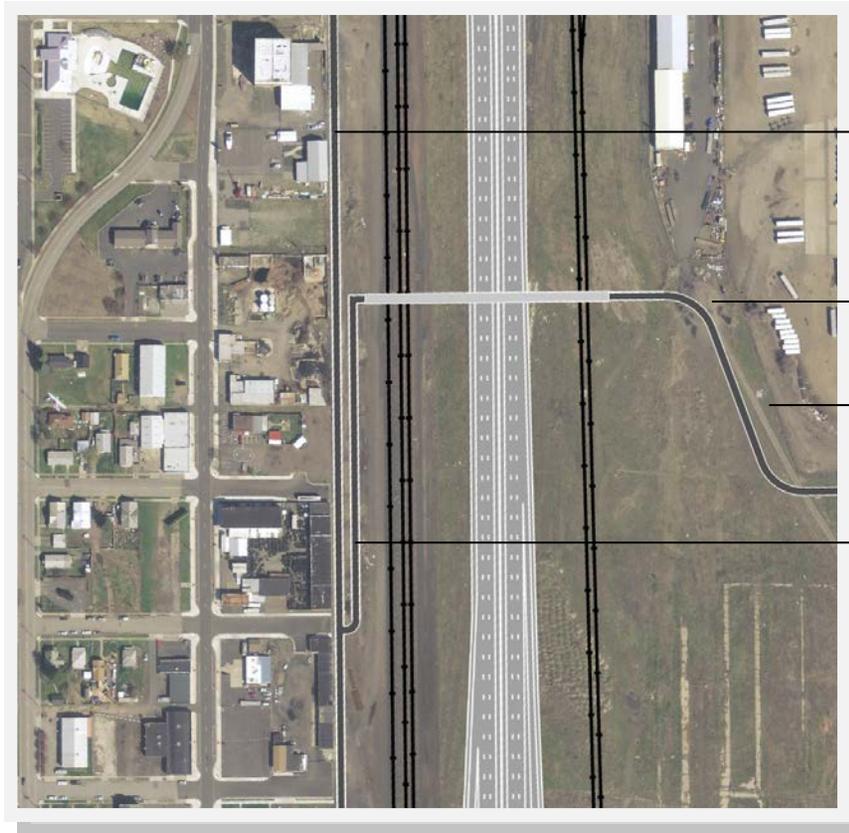
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Neighborhood Locale Design Elements

TENTATIVE:

Rowan Avenue Pedestrian Bridge & The Mural Wall



Children of the Sun Trail

Tentative Pedestrian Bridge

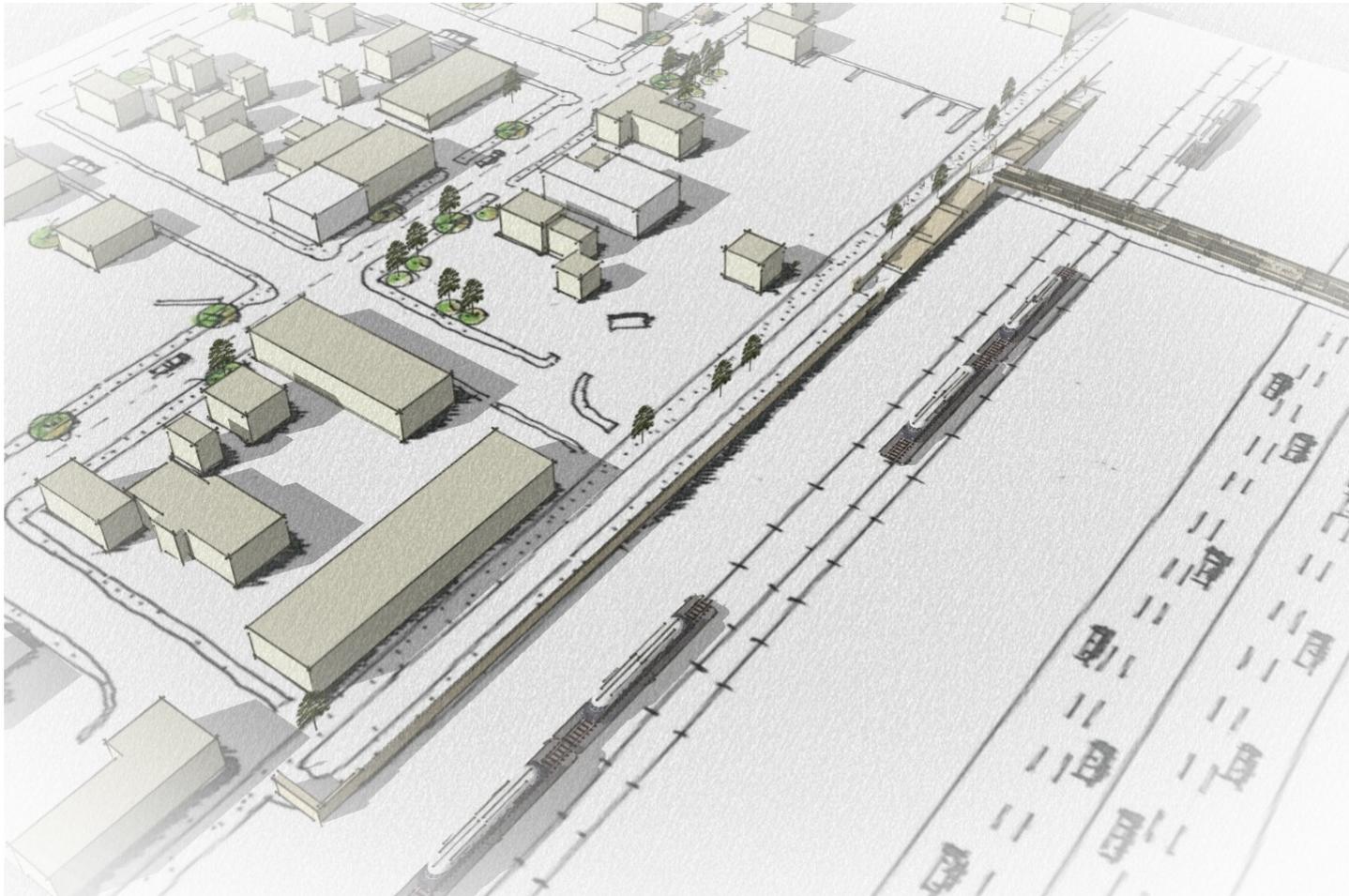
Retaining Walls and Bridge Ramps

Tentative Mural Wall integrated with the bridge approach ramps

Note: WSDOT will continue to work with the neighborhood councils to incorporate mutually agreeable details into the design where possible and within *the policies that guide our work and available funding.*

Bemiss, Hillyard, Minnehaha, Chief Garry & East Central Neighborhoods

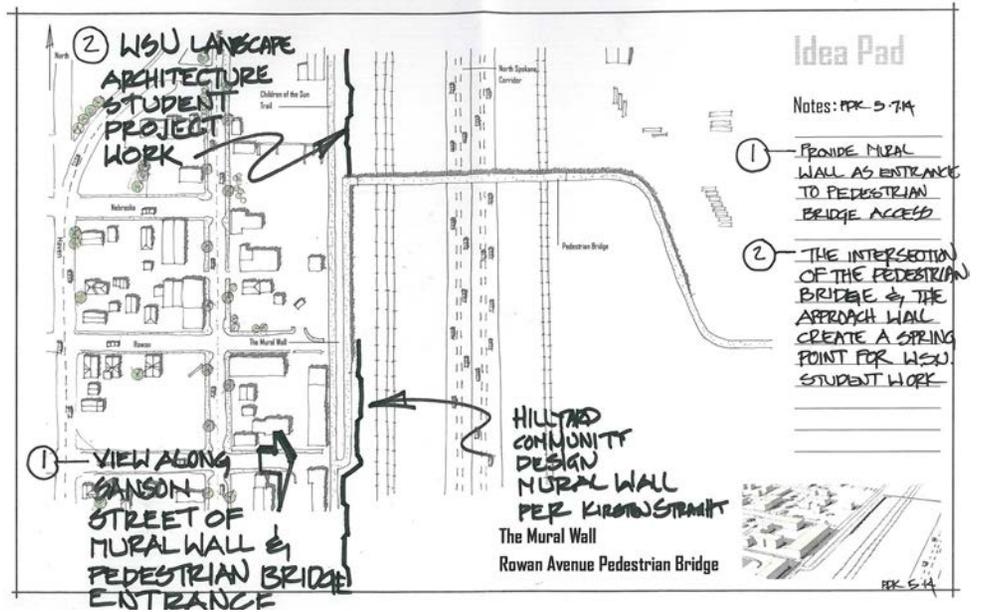
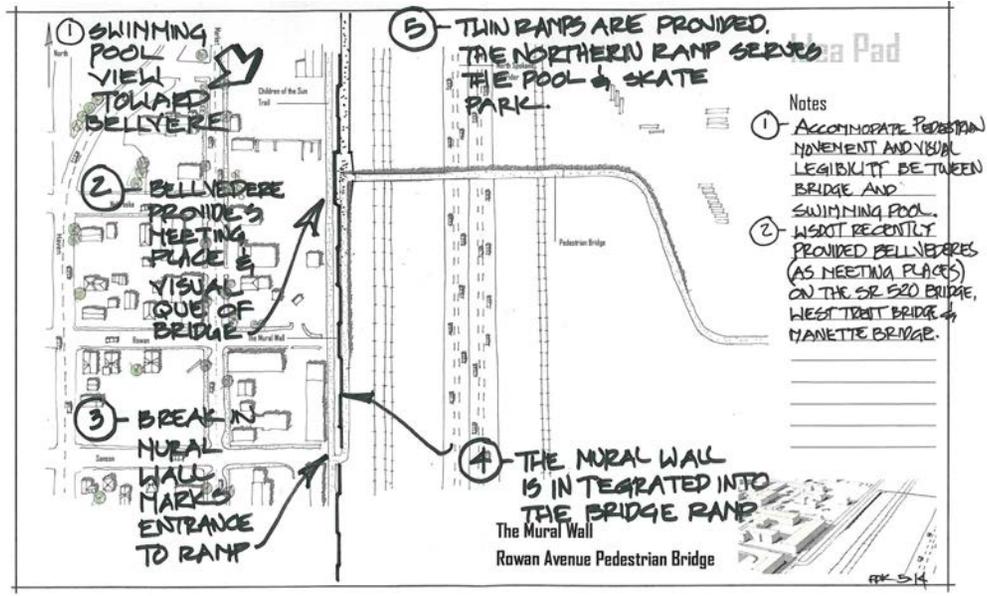
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Tentative Rowan Avenue Pedestrian Bridge
Tentative Mural Wall
3D Computer Study Model

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Note: The pedestrian bridge is tentative and pending agreements with stakeholders.

Post Workshop I Design Team Summaries

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TENTATIVE: Rowan Avenue Pedestrian Bridge & The Mural Wall

Recommendations



Provide Belvederes for a high quality pedestrian visual experience. Overlooks aid in “natural surveillance” and a feeling of public protection.



Accommodate the Mural Wall design for the Hillyard Neighborhood’s concept.



Build a unique and vivid bridge to aide pedestrian “way-finding” and walkability.

AESTHETIC GOALS

Create an inviting pedestrian experience.

Encourage trail use to increase pedestrian presence for greater safety.

AESTHETIC CHARACTER

- Materials blending with Hillyard’s existing structures.
- Pedestrian scale
- Displays of community sponsored murals.

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TENTATIVE:

Rowan Avenue Pedestrian Bridge & The Mural Wall

Recommendations



Provide Pine Trees
throughout the corridor.



Plant with Native Grasses
wherever possible.



Use Lilac bushes where appropriate.

AESTHETIC GOALS

Express the local
identity with native
pines and grasses .

The landscape design
should appear as
though its always been
there.

AESTHETIC CHARACTER

- Naturalistic

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Rowan Avenue

TENTATIVE:

The Mural Wall Study

View looking south along the mural wall.



The Mural Wall design features a canvas for the Hillyard Neighborhood's art project.

Wall concepts show an integrated design that includes the bridge ramps and landscape features.

The preliminary sketches are produced prior to the Hillyard Neighborhood's final mural design.

The WSU School of Landscape Architecture projects will inform the final design.



View looking east with the pedestrian bridge behind the mural wall.

Form Study

AESTHETIC GOALS

Provide inviting views of the mural wall looking east along the side streets of the Hillyard business area.

Encourage trail use and obscure views of NSC traffic.

AESTHETIC CHARACTER

- Pedestrian scale
- Displays of community sponsored murals
- Materials blending with established Hillyard buildings

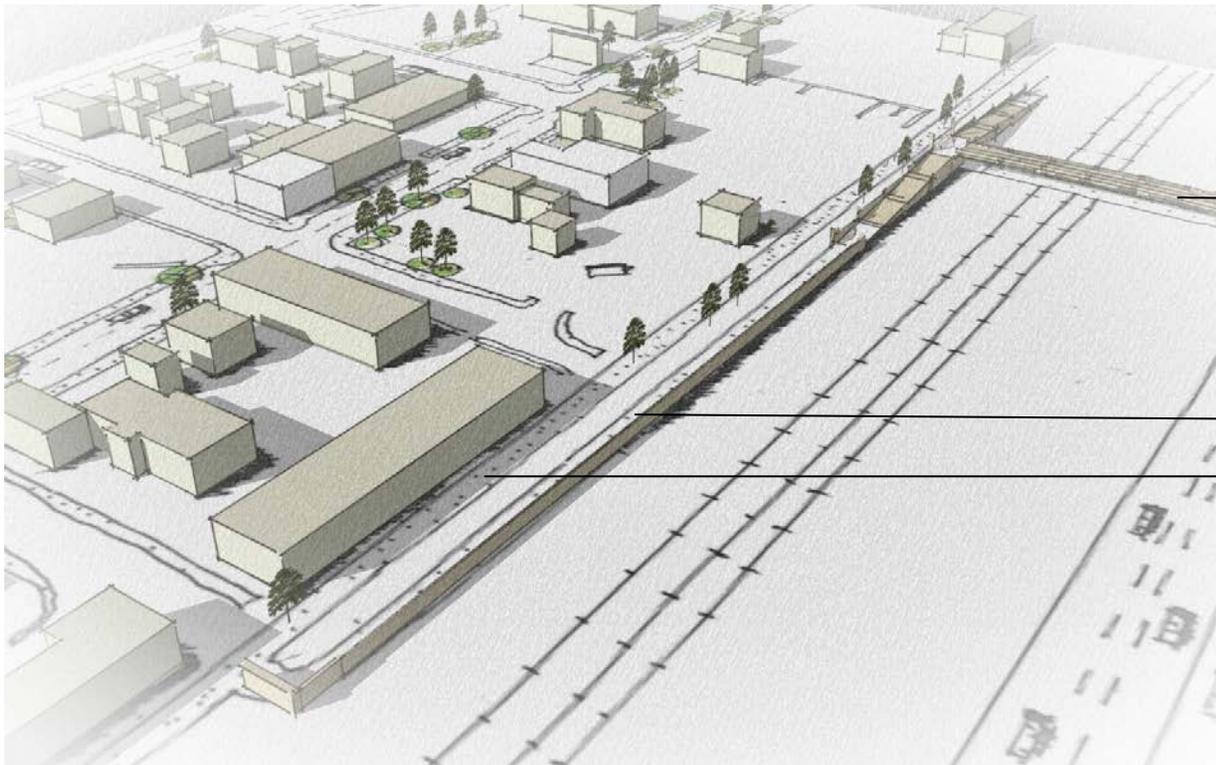
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Rowan Avenue

TENTATIVE:

The Mural Wall



Discussion:

The west bridge entrance should have both north and south access.

The mural wall should be an open public space visible from adjacent streets.

Potential pedestrian bridge

The mural wall.

The Children of the Sun Trail

Birds eye view looking north with the potential bridge in the upper right corner.

The proposed mural wall is adjacent to the Children of the Sun Trail.

Form Study

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Rowan Avenue

TENTATIVE:

The Mural Wall



Plan view with the bridge oriented east and west.

The mural wall and potential bridge ramps. An alternate route may be accommodated with an additional stairway and bicycle gutter.

Potential bridge.



Discussion:

The north access serves the pool and park to the north-west.

The ramp to the south serves the main entrance and focal beginning of the mural wall.

Form Study

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Rowan Avenue

TENTATIVE:

The Mural Wall Study



Discussion:

The Mural Wall provides a framework for the Hillyard Neighborhoods art project.

The design will evolve as engineering proceeds.

Designers should reference the work of WSU landscape architecture students.



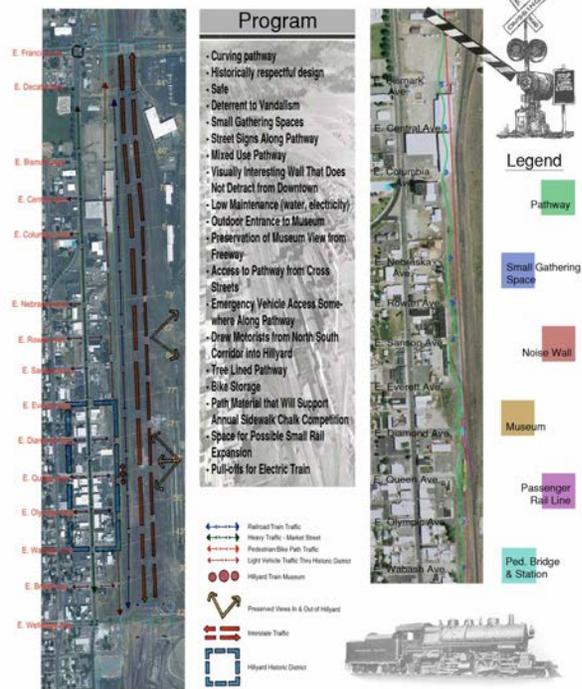
Designers are anticipating a formal entrance to the mural wall.

Form Study

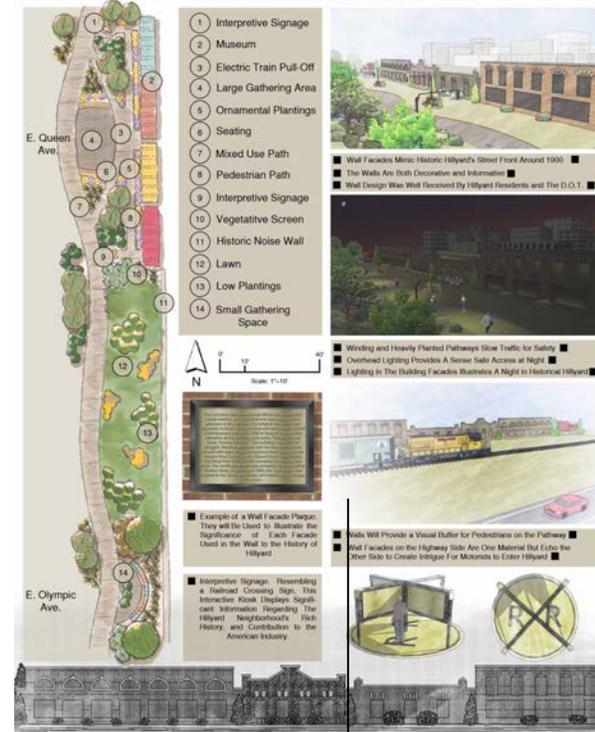
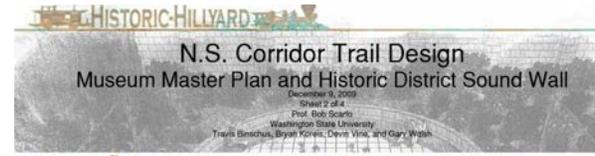
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Rowan Avenue TENTATIVE: The Mural Wall Study



Site



The Museum

Discussion:

The Mural Wall provides a physical framework for the Hillyard Neighborhoods art project.

WSU landscape architect students have work produced in collaboration with the neighborhood.

These ideas will be accommodated to the greatest extent possible during final design.

Character Study

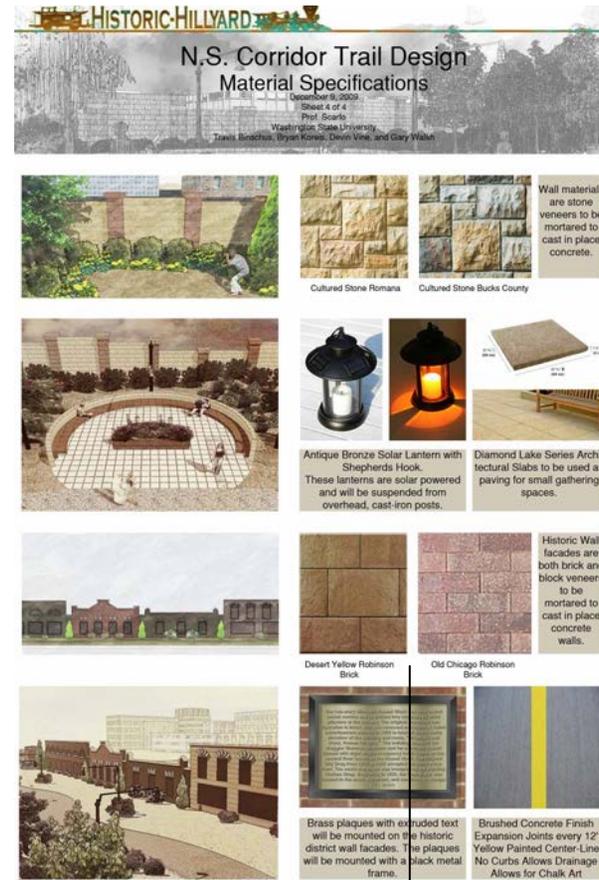
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Rowan Avenue TENTATIVE: The Mural Wall Study



People Places



Enduring Materials

Discussion:

WSU landscape architect students have produced classical ideas for making high quality public spaces. These have been popular with neighborhood leaders.

Gathering places, interesting streets and enduring materials produce safe public areas.

The design team will reinforce these principles during final design.

Character Study

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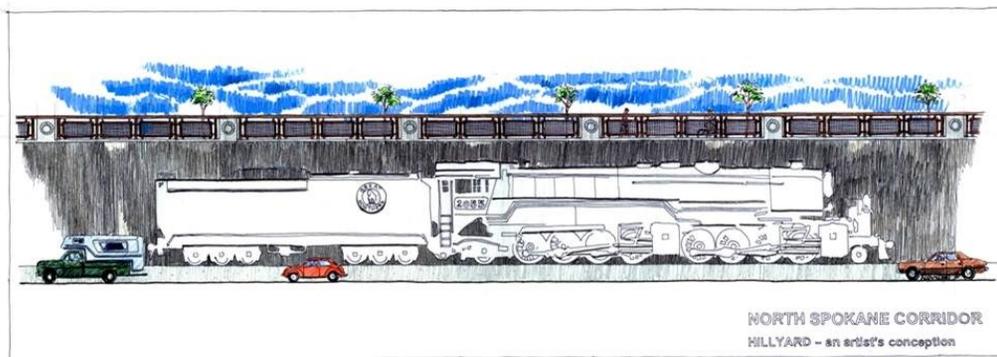
Rowan Avenue TENTATIVE: The Mural Wall Study



Discussion:

The WSU student work is supported by these early NSC designs from historic archives.

Neighborhood character should be referenced in architectural designs.



Character Study



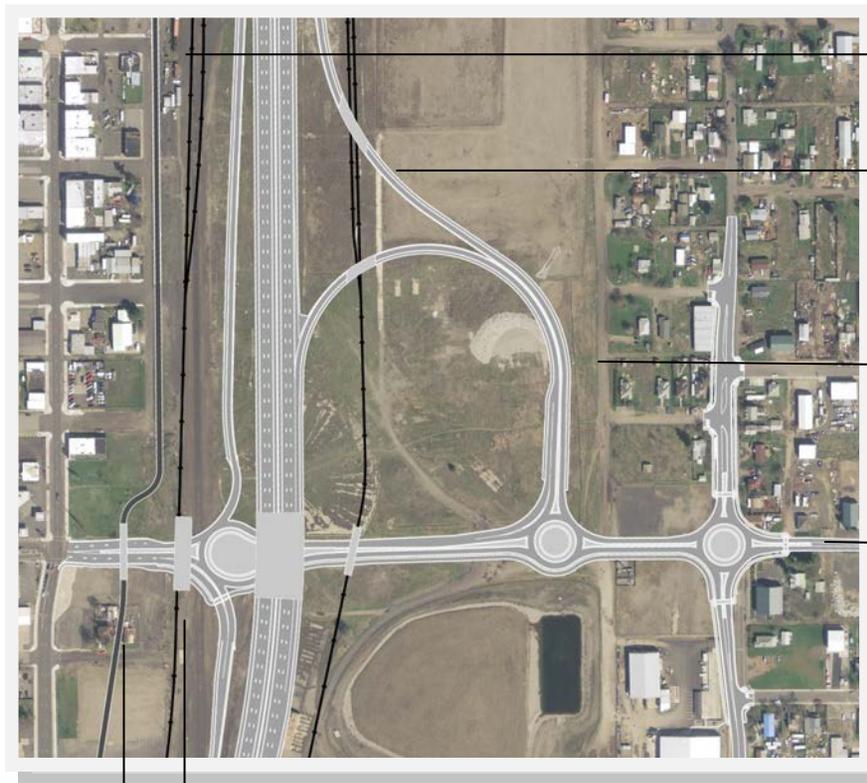
Early NSC design studies from project archives are based on referencing historic neighborhood buildings .

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Neighborhood Locale Design Elements

Wellesley Avenue



Bicycle Pedestrian Trail

NSC travelers ramp Bridges over railroad

Retaining Walls

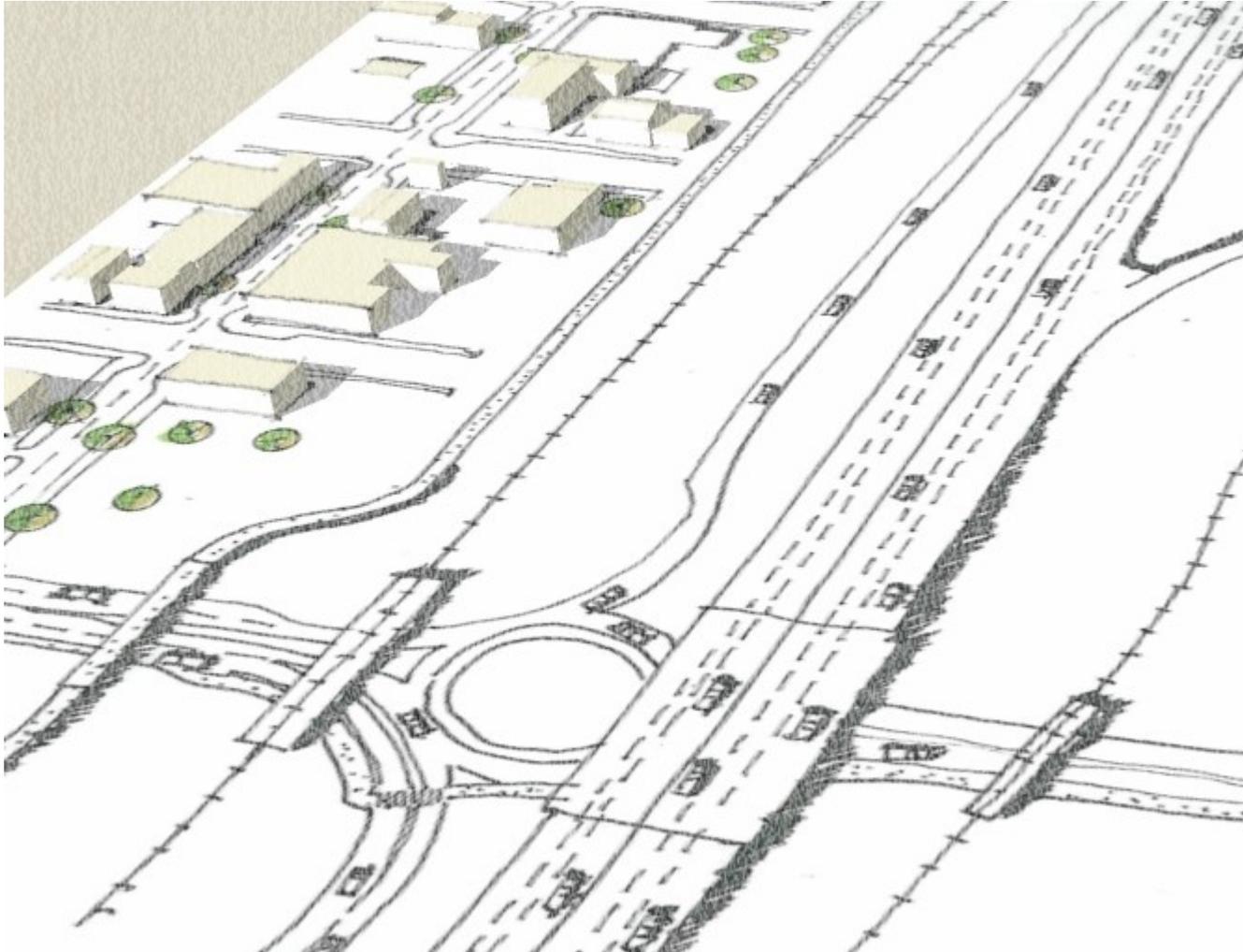
Roundabouts

Railroad Bridges over Wellesley

Pedestrian Bridge over Wellesley (Optional)

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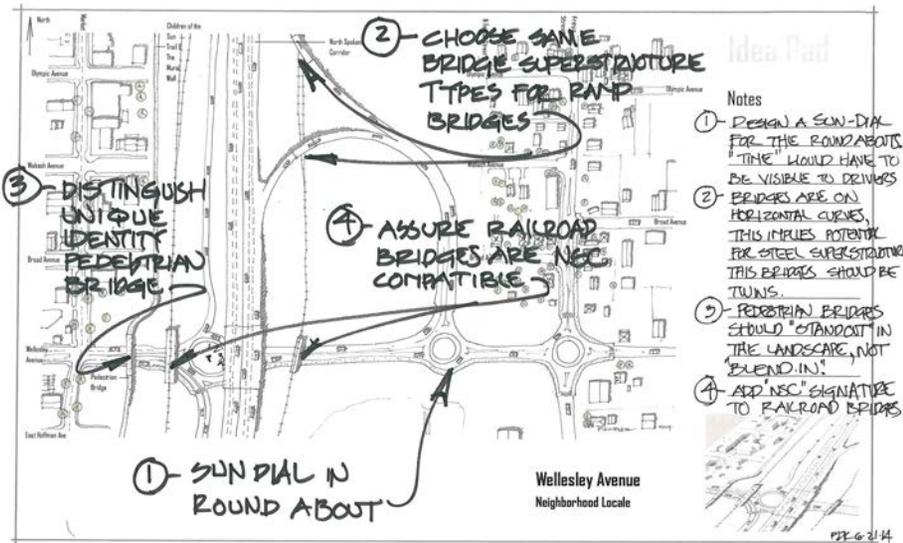
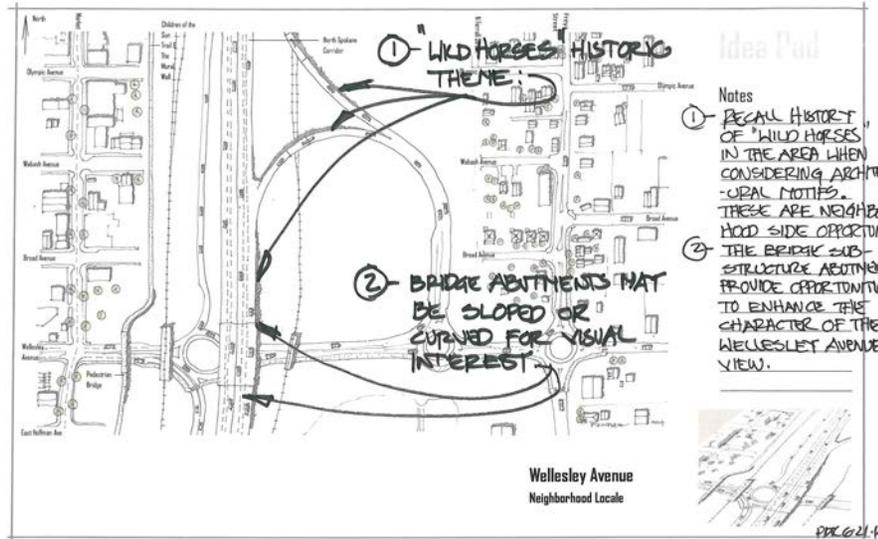


Wellesley Avenue

3D Computer Study Model

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Post Workshop I Design Team Summaries

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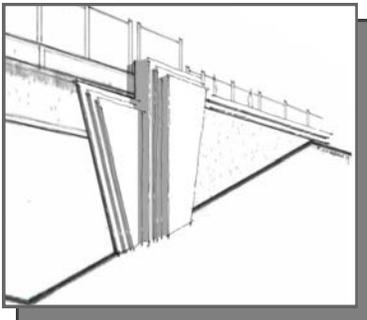
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Wellesley Avenue

Recommendations



Assure that the Railroad Bridge details are compatible with the Wellesley Avenue bridges.



Design the bridge abutments with sloping abutment wall forms. These will be 'retro' designs and unique to corridor.



Provide Pedestrian Bridges with a unique identity at each location. They should be designed not only for conveyance, but also as a means for a visual experience.

AESTHETIC GOALS

Create a gateway for NSC travelers entering the Hillyard neighborhood.

Bridges in one unified style as seen from Wellesley Avenue.

Designs unique to the neighborhood as opposed to copies from elsewhere.

AESTHETIC CHARACTER

- Modernistic
- Somewhat 'retro'

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Wellesley Avenue

Recommendations



Integrate motifs referencing the regions history of wild horses.



Design focal points in roundabouts such as sun dials or basalt with native grasses.



AESTHETIC GOALS

Encourage trail and sidewalk pedestrian use. The presence of more people leads to greater safety.

Increase 'traffic calming'.

Reinforce local history of wild horses.

AESTHETIC CHARACTER

- Naturalistic
- Interesting focal points

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Wellesley Avenue

Recommendations



Continue planting pines in this part of the corridor.



Use native grasses in selected locations, such as roundabouts.



Plant lilacs since Spokane is the 'Lilac City'.

AESTHETIC GOALS

Create a unified landscape design with interesting focal points.

Roundabouts should be unique to Wellesley Avenue.

AESTHETIC CHARACTER

- Naturalistic
- Seasonally colorful focal points

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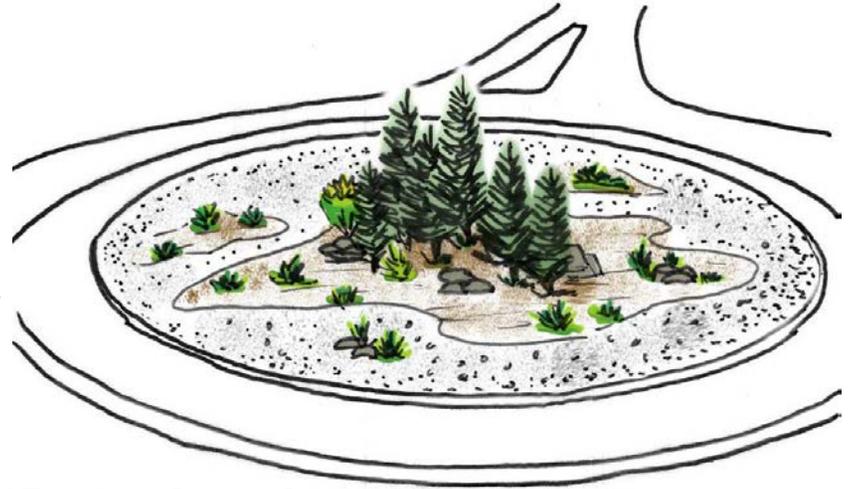
Wellesley Avenue

Roundabout Study

Note: Concepts A & B may be considered cost effective designs provided all plants are able to thrive without permanent irrigation. Concept C, with ornamental plants, permanent irrigation and public art is beyond cost effective design, requiring additional funding by others through agreement.

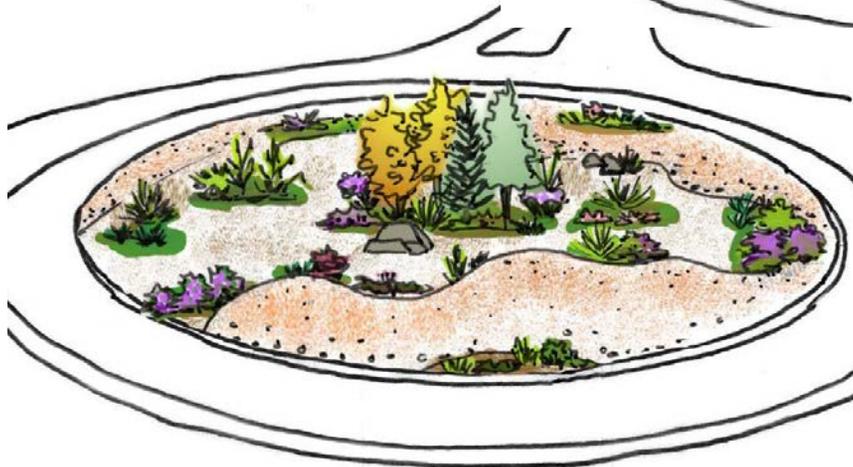
Landscape Concept **A**

NATIVE: Low maintenance native plants, native grasses, basalt boulders, river rock or gravel



Landscape Concept **B**

NATIVE/NON-NATIVE MIX: Mix of native and ornamental plants, basalt boulders, river rock



Landscape Concept **C**

ORNAMENTAL: Ornamental plants, basalt boulders, decorative rock, art feature such as a sundial

