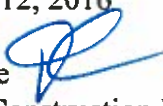




DATE: December 12, 2016

TO: Derek Case   
Assistant Construction Engineer  
Headquarters, Mail Stop 47354

THRU: Julie Meredith/Dave Becher <sup>DPB</sup>  
Program Administrator, SR 520 Program, Mail Stop NB82-99

FROM: Stephen Strand  
Project Engineer, West Approach Bridge North Project,  
Mail Stop TB-93, (206) 770-3518

PROJECT: C-8625  
SR 520 West Approach Bridge North Project

SUBJECT: **CO # 72 – Marine Casing Ext and Misc**

**Requested Action:**

Review     Review and Region Execution     Review and HQ Execution

**Change Approval**

<b>Dpty. Construction Mgr. Concurrence:</b>	James VanSteenburg	Various*
<b>Project Engineer Concurrence:</b>	Steve Strand	Various*
<b>Project Engineer Change Approval:</b>	Brian Dobbins	Various*
<b>Region Change Approval:</b>	Dave Becher	Various*
<b>HQ Construction Change Approval:</b>	Derek Case	Various*
<b>Office of Equal Opportunity (OEO):</b>	John Huff	Various*
<b>FHWA Approval:</b>	Anthony Sarhan	Various*
<b>Program Funding Concurrence:</b>	Janet Buoy	9/21/2016

\* See Attachment A for details regarding Change Approvals and Approvals to Proceed.

**Description of the Change**

This change order includes several independent changes that have or will occur on the West Approach Bridge North (WABN) project including:

- **Marine Casing Extensions**  
The Contract Plans detail the elevation that the permanent shaft casings for the WABN structure are to be advanced into dense soil. During construction there were several instances where casings began to “sink” after being advanced to the specified design elevation leading to the conclusion that the plan elevations were not sufficient. This WSDOT

initiated change order extends the permanent shaft casings for Piers 34-41 to ensure they are properly seated in dense soil.

This change order also resolves the following Contract changes that will no longer be processed independently:

- **ITS Mods From Damaged SB (Draft CO #78)**  
An accident on SR 520 resulted in the destruction of Sign Bridge No. 520/1.7E (SB00404). WSDOT determined that an existing Dynamic Message Sign (DMS) can be utilized while still maintaining an acceptable level of service without replacing SB00404 as required in the Contract. This WSDOT initiated change order deletes Contract Work that no longer needs to be completed as a result of the unanticipated removal of SB00404.
- **Work Platform Screening Mod (Draft CO #61)**  
The Contract Plans specify a grating screen to be installed on the south side of the equipment platform belvederes at Piers 16, 21, 28 and 36. This WSDOT directed change replaces this screen with railing and toe plate for improved access for maintenance.

In addition to the changes above and as a part of this change order, the Contractor has agreed to retract their protests of the following:

- **Tire Wash - FWI SL #0276 & SL #0281 (See Attachment C)**  
The Contract Plans, Provisions, and Water Quality Monitoring & Protection Plan (WQMPP) detail Temporary Erosion & Sediment Control (TESC) measures that must be implemented during construction. The WQMPP requires a tire wash at the entrance/exit point of the Contractor's Temporary Work Access (Trestle) until the Trestle is removed. The Contractor may propose modifications to the WQMPP, but they must be approved by the Department of Ecology.

The Contract Staging Plans direct the Contractor to install the tire wash during stage 1 of construction, but the tire wash is not shown in stage 2 of construction. The Contractor's position is that the tire wash is no longer required during stage 2 of construction as the tire wash is not shown on the stage 2 plans. In addition to the Contractor's stance regarding the tire wash as shown in the staging plans, the Contractor also states that WSDOT delayed contract work in the vicinity of Wall 1 due to taking too long to approve Contractor proposed modifications to the WQMPP which would allow hose stations in lieu of a tire wash.

- **Falsework - FWI SL #0222 & SL #0231 (See Attachment C)**  
The Contract requires that the Contractor design and construct falsework for the WABN structure and provides design standards that the falsework must adhere to. A falsework submittal was transmitted to WSDOT for approval that would utilize the permanent casings to support the falsework. WSDOT rejected the falsework submittal as there are no

applicable design standards which support the Contractor's stance that there is sufficient bond strength between the permanent casings and columns to support the falsework holding Bridge girders and diaphragms. The Contractor is seeking compensation for the additional cost of labor and materials to attach the falsework to the top of the Bridge shafts, not the permanent casings.

**Evolution of the Change**

**Extended Marine Casings**

On October 1, 2015, a WSDOT inspector observed the casing at Pier 36 Shaft B sinking after initial installation. This issue was reviewed and on October 5, 2015, WSDOT SL #222 directed the Contractor to extend the casing an additional seven feet to an elevation of -21.0.

On October 7, 2015 and October 8, 2015, permanent shaft casings were installed for Pier 38 Shaft B and Pier 39 Shaft B. Measurements were taken and materials tested and a determination made that these casings would also need to be extended. On October 9, 2015, WSDOT SL #223 directed the Contractor to extend the casings ten and seven additional feet, respectively, for these shafts. Revising the elevation to -30.0 feet for Pier 38 Shaft B and -16.0 for Pier 39 Shaft B.

On October 28, 2015, the Contractor submitted FWI SL #180 providing notice of settlement of the permanent casing for Pier 35 Shaft B. On October 29, 2015, WSDOT SL #237 directed the Contractor to extend the casing an additional nine feet, revising the plan elevation to -23.0.

On October 29, 2015, FWI SL #181 provided notice that the casing for Pier 36 Shaft B was sinking and cited differing site conditions (DSC). This settlement occurred after the casing had been extended seven feet per direction provided on October 5<sup>th</sup>, via WSDOT SL #222. Flatiron West, Inc. (FWI) had stopped work on this shaft due to "possible ramifications of the can (casing) sinking any further. This could result in major environmental issues, the weld to the casing extension sliding below the lake level, or damaging the integrity of the shaft."

With this latest issue, WSDOT reviewed the remaining permanent shaft casing installations and determined that conditions to date warranted extending casings for Piers 35-41 to mitigate risks inherent with extending casings once installation had begun and to ensure proper seating of these casings in dense soil. On October 29, 2015, WSDOT SL #239 provided direction as follows to extend the permanent shaft casings:

<b>Pier</b>	<b>Shaft</b>	<b>Revised Elevation</b>	<b>Previous Direction</b>
34	A	(-30 feet)	
35	A	(-32 feet)	
35	B	(-32 feet)	WSDOT SL #237 (-23 feet)

36	A	(-34 feet)	
36	B	(-31 feet)	WSDOT SL #222 (-21 feet)
37	A	(-35 feet)	
37	B	(-35 feet)	
38	A	(-33 feet)	
38	B	(-33 feet) +	WSDOT SL #223 (-30 feet)
39	A	(-27 feet)	
39	B	(-27 feet)	WSDOT SL #223 (-16 feet)
40	A	(-27 feet)	
40	B	(-27 feet)	
41	A	(-39 feet)+	
41	B	(-35 feet)	

Change Orders 68 and 107 were previously executed in order to make timely payment to the Contractor and its subcontractors while the overall change order was negotiated. Those amounts have been deducted from the settlement amount.

#### **ITS Mods From Damaged SB**

On March 17, 2015, an accident on SR 520 resulted in the demolition of sign bridge SB00404. The Contract calls for the removal of the existing signing on SB00404 to be replaced with a Variable Message Sign (VMS). Instead of requiring the Contractor to replace SB00404, it was decided that the existing DMS to be removed by the Contractor located 750 feet east of SB00404 could be utilized without replacing SB00404 or installing the new VMS. WSDOT then initiated this change order to delete from the Contract the removal of the existing DMS and the work required to install the new VMS on SB00404.

This reduction in work reduced the overall price of this change order. Change approvals were obtained as documented in Attachment A.

#### **Work Platform Screening Mod**

On August 21, 2015, the Contractor submitted Request for Information (RFI) 276 Equip Plat Vertical Grating Screen requesting confirmation that vertical grating screen is required on the south side of the equipment platform and requesting details for this work since none were provided. After reviewing the RFI, WSDOT determined that a screen was not necessary on the south side and that a railing was preferred to improve access to this area by maintenance personnel. This reduction in work reduced the overall price of this change order. Change approvals were obtained as documented in Attachment A.

#### **Tire Wash**

On June 16, 2016, the Contractor proposed several modifications to the WQMPP including a proposal to use hose stations instead of a tire wash at the entrance to the Trestle and a request to eliminate the need to backfill the removed piles with Bentonite clay. The Contractor had previously requested that the tire wash requirement be eliminated in RFI 056, but the proposal was rejected because the request did not provide sufficient information that the alternate method is equal to or better than a tire wash. The Department of Ecology and Department of Fish and Wildlife have differing rules regarding the use of Bentonite clay for in-water

work, and a response to the WQMPP revision proposal was not immediately provided.

On July 21, 2016, the Contractor notified WSDOT via email that stage 2 of construction would begin on July 29, 2016 and the tire wash stations would be removed in accordance with the Contract Staging Plans. The Contractor stated in the email that if WSDOT wants the tire wash left in place through stage 2, there would be cost and schedule impacts because the Contractor could not perform Wall 1 work with the tire wash in place. The Contractor pointed out that no response had yet been provided regarding the use of hose stations at the Trestle.

WSDOT responded to the email in WSDOT SL #0421 on July 29, 2016 stating that the Contractor had not provided sufficient details that the hose stations were equal to or better than the tire wash detailed in the Contract Provisions. WSDOT met with the Contractor and Department of Ecology and approval to use hose stations instead of a tire wash was provided and hose stations installed on August 6, 2016. The Contractor submitted FWI SL #0276 on August 2, 2016 and SL #0281 on August 15, 2016 providing their notice of protest and supplement to that protest. The Contractor seeks compensation to recover time lost between July 29, 2016 and August 6, 2016.

This change order eliminates a protest by the Contractor and settles any potential additional costs to WSDOT.

#### **Falsework**

The Contractor submitted S.167.004 Falsework for Piers 19-40 for WSDOT review in August 2015. The submittal was approved as noted, as the Contractor was not able to establish sufficient evidence showing that the bond strength between the permanent shaft casings and shaft concrete would support the Bridge falsework. In subsequent submittals regarding the Bridge falsework, the Contractor failed to prove that their assumptions were correct regarding the bond strength between the permanent casings and shafts to WSDOT's satisfaction. The Contractor protested WSDOT determination that their design did not meet Contract requirements in FWI SL #0222 on March 2, 2016 then supplemented their protest in FWI SL #0231 on March 17, 2016. WSDOT found the protest to be invalid and notified the Contractor in WSDOT SL #0324 on March 10, 2016 and in WSDOT SL #0335 on March 24, 2016.

This change order eliminates a protest by the Contractor and settles any potential additional costs to WSDOT.

#### **Prior Approval**

##### **Extended Marine Casings**

In order to avoid delays to the schedule and cost impacts associated with resources standing by, prior approval in advance of an executed change order was provided by the Project Engineer as follows:

<b>Date</b>	<b>Doc #</b>	<b>Direction</b>
10/5/15	WSDOT SL #222	Extend Casing for Pier 36B
10/9/15	WSDOT SL #223	Extend Casing for Pier 38B & 39B
10/29/15	WSDOT SL #237	Extend Casing for 35B
10/29/15	WSDOT SL #239	Direction to Extend Casing for Piers 34-41

Change approval and Prior Approval are documented for initial direction for 35B, 36B, 38B, and 39B through the WSDOT serial letters noted above. Change approval to proceed with additional casing extensions were received as shown above and direction provided by the Project Engineer on behalf of HQ Construction via WSDOT SL #239. See Attachment A for approvals documentation.

#### **ITS Mods From Damaged SB**

To ensure that the Contractor did not begin procurement of expensive ITS equipment for work that will no longer occur, WSDOT directed the Contractor to delete the ITS work included in this change prior to being issued an executed change order in WSDOT SL #0303 on February 4, 2016. See Attachment A for approvals documentation.

#### **Work Platform Screening Mod**

To ensure that the Contractor did not begin procurement of expensive steel grating in lieu of bridge railing for their work platform through a WSDOT initiated change, WSDOT directed the Contractor to replace the steel grating with railing prior to being issued an executed change order on February 22, 2016. See Attachment A for approvals documentation.

#### **Payment**

In accordance with Standard Specifications 1-04.4, the Contractor is entitled to compensation for the additional cost of the labor, material and equipment as a result of this change, and WSDOT is entitled to cost savings for the reduction in scope of work. WSDOT and FWI negotiated a \$4,990,000 settlement to cover all of these issues. The (net) total negotiated lump-sum cost of this change order is \$2,890,000 after accounting for previous interim change order payments made through Change Orders 68 and 107. All items contained in this change order are full and final settlement. See the table below for a summary of all costs associated with this change order.

Total negotiated price for all work covered by this change order	\$4,990,000
CO #68 Interim Payment	(\$1,500,000)
CO #107 Interim Payment	(\$600,000)
<b>CO #72 Remaining Balance</b>	<b>\$2,890,000</b>

See Attachment B for the engineer's estimate.

#### **Time**

The casing installation work modified in this change order was not on the schedule critical path. However, the duration of the casing installation was extended 45 days beyond the date that the Contractor planned to have completed installation of the marine casings. The Contractor is entitled to compensation for labor, equipment, and material costs due to the extended, unplanned duration of that work and their efforts in recovering some of the added time through overtime and added resources. Had the contractor not pursued the work as efficiently as they did the critical path may have been impacted. They are not entitled to additional contract time since the overall Project schedule critical path was not impacted. See Attachment B for further details.

Contract time is not affected by this change order.

**DBE Statement**

This change order transfers DBE participation by Amaya to Grady Excavating, Inc. and adds \$540,000 of DBE participation to Rebar International, Inc. which was erroneously reduced during processing of Change Order 73. Amaya's reduced DBE participation as a result of the deleted work due to damaged SB00404 is substituted by Grady Excavating, Inc. The revised Project COA DBE participation amounts are summarized below.

<b>Subcontractor</b>	<b>Increased/Decreased Amount per this CO</b>	<b>Revised Total DBE Amount</b>
Grady Excavating, Inc.	\$51,564	\$594,890.51
Amaya	(\$51,564)	\$1,848,436.00
Rebar International, Inc.	\$540,000	\$12,583,778.86

See Attachment A for approvals of the substitutions.

**Attachments**

- CCIS Change Order Document (22 pages)
- Change Order Checklist (2 pages)
- Change Approval Emails (Attachment A)
- Engineer's Estimate (Attachment B)
- Letters (Attachment C)

File: CO Files: CO 072; ProjectWise: 16.05.072

**WASHINGTON STATE  
DEPARTMENT OF TRANSPORTATION  
CHANGE ORDER**

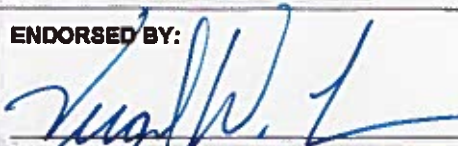
DATE: 11/14/16  
Page 1 of 22

CONTRACT NO: 008625 FEDERAL AID NO: BR-NHPP-0520 (053)  
CONTRACT TITLE: SR 520, MONILAKE TO EVERGREEN PT. BRIDGE WEST APPR  
CHANGE ORDER NO: 72 MARINE CASING EXT AND MISC

PRIME CONTRACTOR: SW0178155 FLATIRON WEST, INC  
18702 NORTH CREEK PARKWAY #202  
BOHELL WA 98011-8019


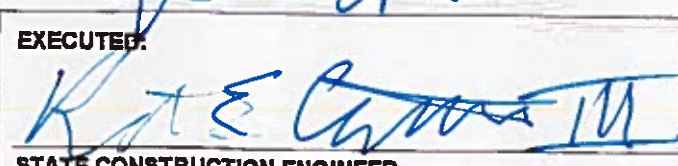
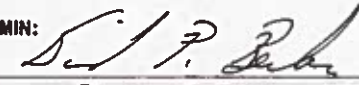
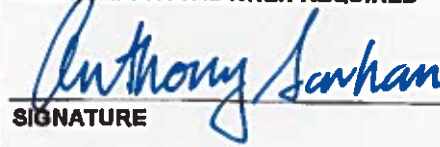
Ordered by Engineer under the terms of Section 1-04.4 of the Standard Specifications

Change proposed by Contractor

ENDORSED BY:  CONTRACTOR 12/8/2016 DATE	SURETY CONSENT:  ATTORNEY IN FACT  DATE
--	---

ORIGINAL CONTRACT AMOUNT: 199,537,370.50  
CURRENT CONTRACT AMOUNT: 202,601,632.00  
ESTIMATED NET CHANGE THIS ORDER: 2,890,000.00  
ESTIMATED CONTRACT TOTAL AFTER CHANGE: 205,491,632.00

Approval Required:  Region  Olympia Service Center  Local Agency

<input checked="" type="checkbox"/> APPROVAL RECOMMENDED  PROJECT ENGINEER 12-13-16 DATE	<input type="checkbox"/> EXECUTED  STATE CONSTRUCTION ENGINEER 12/19/16 DATE
<input checked="" type="checkbox"/> APPROVAL RECOMMENDED REGIONAL ADMIN:  BY: SR 520 Director of Construction December 18, 2016 DATE	<input type="checkbox"/> EXECUTED OTHER APPROVAL WHEN REQUIRED  SIGNATURE 12/19/16 DATE FHWA REPRESENTING CAPS & CCIS to 12/16/2016



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CONTRACT NO: 008625

CHANGE ORDER NO: 72

All work, materials, and measurements to be in accordance with the provisions of the Standard Specifications and Special Provisions for the type of construction involved.

This contract is revised as follows:

**Description:**

This change order makes the following modifications to the Contract Plans and Provisions:

1. Extend the permanent shaft casings for Piers 34 through 41, to ensure the casings advance adequately into dense soil.
2. Delete Contract Work that is no longer required as a result of the prematurely demolished Sign Bridge No. 520/1.76E (SB00404) and deletes the requirement for the removal of the existing Dynamic Message Sign (DMS) in the vicinity of Foster Island. Destruction and replacement of Variable Message Sign (VMS) 520vm00177 was addressed under CO #048 SR520 Closure Costs. Disadvantaged Business Enterprise (DBE) participation for this item is adjusted as detailed below.
3. Replace the galvanized bar grating screen on the south side of the equipment platform belvederes at Piers 16, 21, 28 and 36 with railing and toe plate.

This change order settles the following issues:

4. The Contractor's protest regarding falsework for the WAEN structure as detailed in FWI SL#0222 and FWI SL#0231 is retracted.
5. The Contractor's protest regarding tire wash stations as detailed in FWI SL#0276 and FWI SL#0281 is retracted.

This change order also increases the DBE Condition of Award (COA) commitment amount for bid item #146 performed by Rebar International, Inc. by \$540,000 to correct an erroneous reduction of that same amount in Change Order #73.

**General Criteria:**

The Project COA DBE participation is revised as follows:

1. DBE participation by Grady Excavating, Inc. is increased to \$594,890.51 for work performed under the following bid item:

128	Constructing 9'-0" Diam. Shaft	\$393,632.51
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2. DBE participation by Amaya is decreased to \$1,848,436.00 for work performed under the following bid items:

257	Sign Bridge No. 1	\$148,364.00
258	Sign Bridge No. 2	\$110,709.00
259	Sign Bridge No. 3	\$139,009.00
260	Sign Bridge No. 4	\$102,817.00
261	Sign Bridge No. 5	\$152,205.00
263	Sign Bridge Support Bracket	\$152,205.00
265	Illumination System	\$308,650.00

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266	COS Illumination System	\$800.00
267	COS Traffic Signal System	\$51,450.00
268	COS Traffic Signal System	\$45,430.00
271	Bus Beacon System	\$6,300.00
272	Closed Circuit Television	\$35,742.00
276	Dynamic Message System	\$594,755.00

3. DBE participation by Rebar International, Inc. is increased to \$12,583,778.86 for work performed under the following bid item:

146	St. Rein. Bar For Shaft	\$3,300,313.86
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The Contractor shall assure that subcontracts are executed as part of meeting the DBE Subcontracting Goal that was established as part of the COA.

**Construction Criteria:**

Contract Provisions Volume 1 of 2, Special Provisions Shaft Excavation, Section 6-19.3(3)B Temporary and Permanent Shaft Casing is supplemented as follows:

Permanent casing elevations (Elev.) shall be changed as follows:

Pier 34 Shaft A, Plan Elev.	-14.0, Revised Elev.	-30.0
Pier 35 Shaft A, Plan Elev.	-14.0, Revised Elev.	-32.0
Pier 35 Shaft B, Plan Elev.	-14.0, Revised Elev.	-32.0
Pier 36 Shaft A, Plan Elev.	-19.0, Revised Elev.	-34.0
Pier 36 Shaft B, Plan Elev.	-14.0, Revised Elev.	-31.0
Pier 37 Shaft A, Plan Elev.	-21.0, Revised Elev.	-35.0
Pier 37 Shaft B, Plan Elev.	-19.0, Revised Elev.	-35.0
Pier 38 Shaft A, Plan Elev.	-20.0, Revised Elev.	-45.0
Pier 38 Shaft B, Plan Elev.	-20.0, Revised Elev.	-33.0
Pier 39 Shaft A, Plan Elev.	-9.0, Revised Elev.	-27.0
Pier 39 Shaft B, Plan Elev.	-9.0, Revised Elev.	-27.0
Pier 40 Shaft A, Plan Elev.	-23.0, Revised Elev.	-27.0
Pier 40 Shaft B, Plan Elev.	-23.0, Revised Elev.	-27.0
Pier 41 Shaft A, Plan Elev.	-14.0, Revised Elev.	-35.0
Pier 41 Shaft B, Plan Elev.	-10.0, Revised Elev.	-35.0

Contract Plans Volume 4 of 10 is modified as follows:

Plan Sheets IIN03a titled "Illumination General Notes & Legend", IIR04 titled "Illumination Removal Plan", ILO4 titled "Illumination Plan", IID03 and IID10 both titled "Illumination Details", TSN06 and TSN09 titled "ITS Wiring Schedule", TS07 titled "ITS Plan" and TSD02, TSD16 and TSD17 all titled "ITS Details" are deleted and replaced with pages 7 through 17 of this change order.

Plan Sheets TSD14, TSD26, TSD68 and TSD69 all titled "ITS Details" are deleted.

Plan Sheet SS7 titled "Sign Specifications" is modified as follows:

- Delete line items R130 and R131.

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Plan Sheet SS8 titled "Sign Specifications" is modified as follows:

- Delete line item R143.

Plan Sheet SN04 titled "Sign Plan" is deleted and replaced with page 18 of this change order.

Contract Plans Volume 9 of 10 is modified as follows:

Plan Sheet BA592 "Sign Mounting at Existing Truss Details 1" is supplemented with the following note:

"All work associated with Sign Bridge No. 520/1.76E (SB00404) is deleted. SB00404 was removed following an accident on March 17, 2015 that destroyed this existing sign structure."

Plan sheets BA617 titled "Belvedere and Equipment Platform Belvedere Elevation and Sections" and BA617.1 titled "Belvedere and Equipment Platform Belvedere Framing Plan 2" are deleted and replaced with pages 19 through 20 of this change order.

Add plan sheet BA619.1 titled "Belvedere and Equipment Platform Belvedere Framing Details 2" and included as page 21 of this change order.

Measurement and Payment:

Total amount of this change is \$2,890,000.

Change Order #068 "Casing Ext Interim Pay" and Change Order #107 "Marine Casing Int Pay 2" were executed previously, providing partial/timely payment to the Contractor for the Marine Casing changes described in this change order. That amount, \$2,100,000, has been deducted from the total negotiated amount of \$4,990,000 leaving a balance paid under this change order in the amount of \$2,890,000.

The new pay item, "CO#72 Marine Casing Ext and Misc", in the Lump Sum amount of \$2,890,000, shall be the final and full payment for all additional costs for equipment, labor, tools, materials, engineering, indirect, overhead, and other costs realized by Flatiron West, Inc. and its subcontractors, consultants, and suppliers for all changes included in this change order.

Time:

Contract time is not affected by this change order.

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<b>CONTRACT NO: 008625</b>	<b>CHANGE ORDER NO: 72</b>
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ITEM NO	GROUP NO	STD ITEM	ITEM DESCRIPTION	UNIT MEASURE	UNIT PRICE	EST QTY CHANGE	EST AMT CHANGE
1074 405	01		CO#72 MARINE CASING EXT AND MISC	L.S.	2,890,000.00	1.00	2,890,000.00

-----  
2,890,000.00  
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CONTRACT NO: 008625

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CONTRACTOR NAME/ID	ITEM NO.	C/P	CURRENT COA AMOUNT	REVISED COA AMOUNT	CONTRACTED PROPOSAL AMT	NARRATIVE DESCRIPTION
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GRADY EXCAVATING, INC.

383762164	0128		342,068.51	393,632.51	11,376,250.00	
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Contractor Total

			342,068.51	393,632.51		
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MARLIES CORPORATION DBA AMAYA ELECTRIC

S10891382	0257		188,805.00	148,364.00	140,000.00	
	0258		148,350.00	110,709.00	105,000.00	
	0259		175,450.00	139,009.00	105,000.00	
	0260		140,058.00	102,817.00	88,000.00	
	0261		144,153.00	152,205.00	88,000.00	
	0263		0.00	152,205.00	230,000.00	
	0265		322,000.00	300,650.00	4,200,000.00	
	0266		850.00	800.00	42,000.00	
	0267		52,450.00	51,450.00	118,000.00	
	0268		45,450.00	45,430.00	155,000.00	
	0271		6,500.00	6,300.00	11,000.00	
	0272		35,167.00	35,742.00	83,000.00	
	0276		640,767.00	594,755.00	675,000.00	

Contractor Total

			1,900,000.00	1,848,436.00		
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REBAR INTERNATIONAL

911204188	0146		2,760,313.86	3,300,313.86	7,322,700.00	
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Contractor Total

			2,760,313.86	3,300,313.86		
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<b>CONTRACT NO: 008625</b>			<b>CHANGE ORDER NO: 72</b>			
CONTRACTOR NAME/ID	ITEM NO.	C/P	CURRENT COA AMOUNT	REVISED COA AMOUNT	CONTRACTED PROPOSAL AMT	NARRATIVE DESCRIPTION

	-----	-----
Change Order Total	5,002,382.37	5,542,382.37
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**CONSTRUCTION NOTES (CONTINUATION FROM ILN03)**

102 DURING STAGE 4B, 600V RATED CONDUCTORS SHALL ONLY BE PROVIDED AT SUCH TIME AS THE 15KV CONDUCTORS TO THE FOSTER ISLAND TRANSFORMER ARE DISCONNECTED AND REMOVED. AT THAT TIME, THE 600V CONDUCTORS CAN BE PULLED INTO THE SAME 4" CONDUIT BEING PROVIDED EARLIER FOR THE 15KV CONDUCTORS.




1

103 INSTALL SCHEDULE 40 PVC CONDUIT FOR 600V RATED CONDUCTORS.

2

104 EXISTING LUMINAIRE AND CONDUIT WAS DESTROYED AS A RESULT OF AN ACCIDENT ON MARCH 17, 2015 AND SHALL BE REPLACED.

3











FILE NAME		TIME 12:45:19 PM		DATE 6/7/2016		PLOTTED BY schiccv		DESIGNED BY M. KUNDURU		ENTERED BY M. KUNDURU		CHECKED BY J. YANG & D. SIMS		PROJ. ENGR. D. EDWARDS		REGIONAL ADM. J. MEREDITH		REVISION		DATE		BY		FED.AID PROJ.NO.		LOCATION NO.						 Washington State Department of Transportation		Contract 8625 Change Order #72 Page 8 of 22		PLAN REF. NO. ILN03a		SHEET 576a OF 1797 SHEETS	
								CO#78 - MODS FROM DAMAGED SB 10/27/15 KM		CO#50 ELECTRICAL - LANDSIDE MODS 8/12/15 KM		CO#49 ITS - NEMABEAONLAYOUT MOD 8/12/15 KM																											

Contract 8625  
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ILLUMINATION REMOVAL NOTES

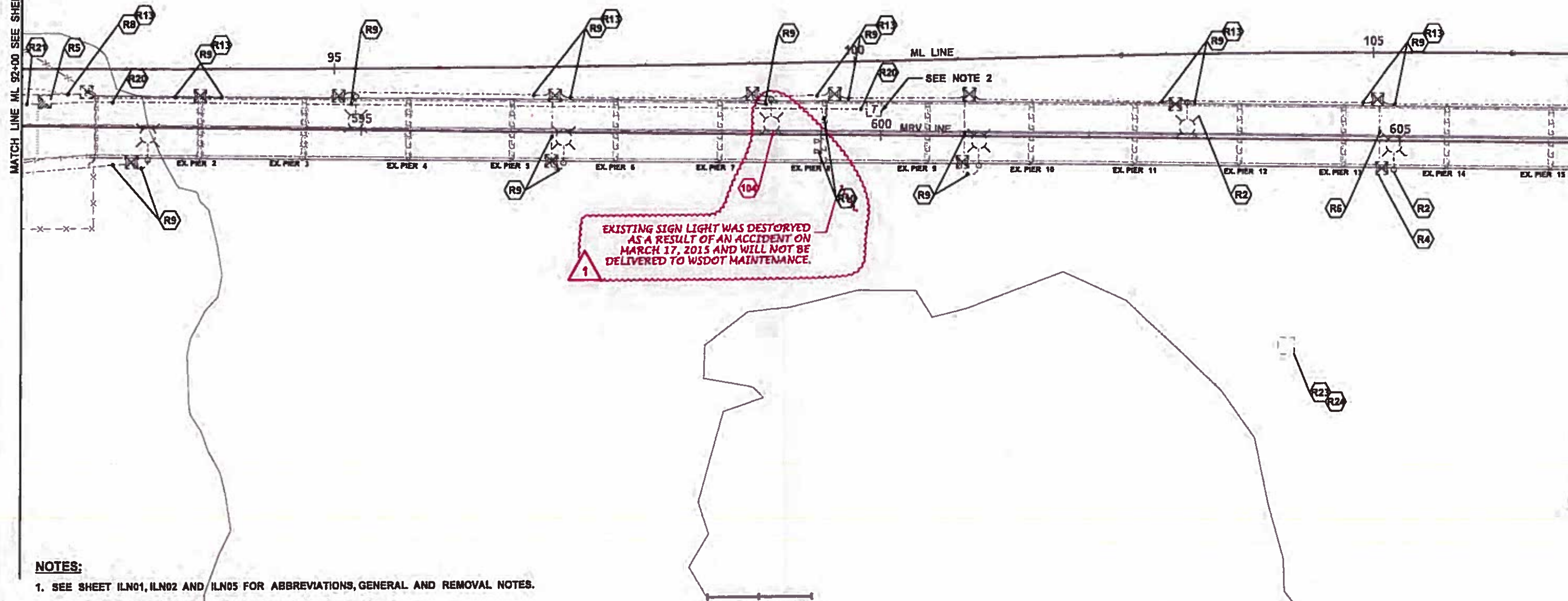
# SEC. 22, T.25N. R.4E. W.M.

## LEGEND:

- EXISTING**
-  WSDOT LIGHT STANDARD WITH SINGLE MAST ARM
  -  WSDOT LIGHT STANDARD WITH DUAL MAST ARM
  -  UNDERDECK LUMINAIRE
  -  WSDOT TYPE E SERVICE CABINET
  -  CONDUIT AND WIRING
  -  WSDOT STD TYPE 1 JUNCTION BOX
  -  WSDOT STD TYPE 2 JUNCTION BOX
  -  WSDOT STD TYPE 4 JUNCTION BOX
  -  NEMA JUNCTION BOX
  -  TRANSFORMER CABINET




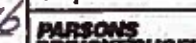

MATCH LINE ML 92+00 SEE SHEET ILR03

MATCH LINE ML 107+00 SEE SHEET ILR05



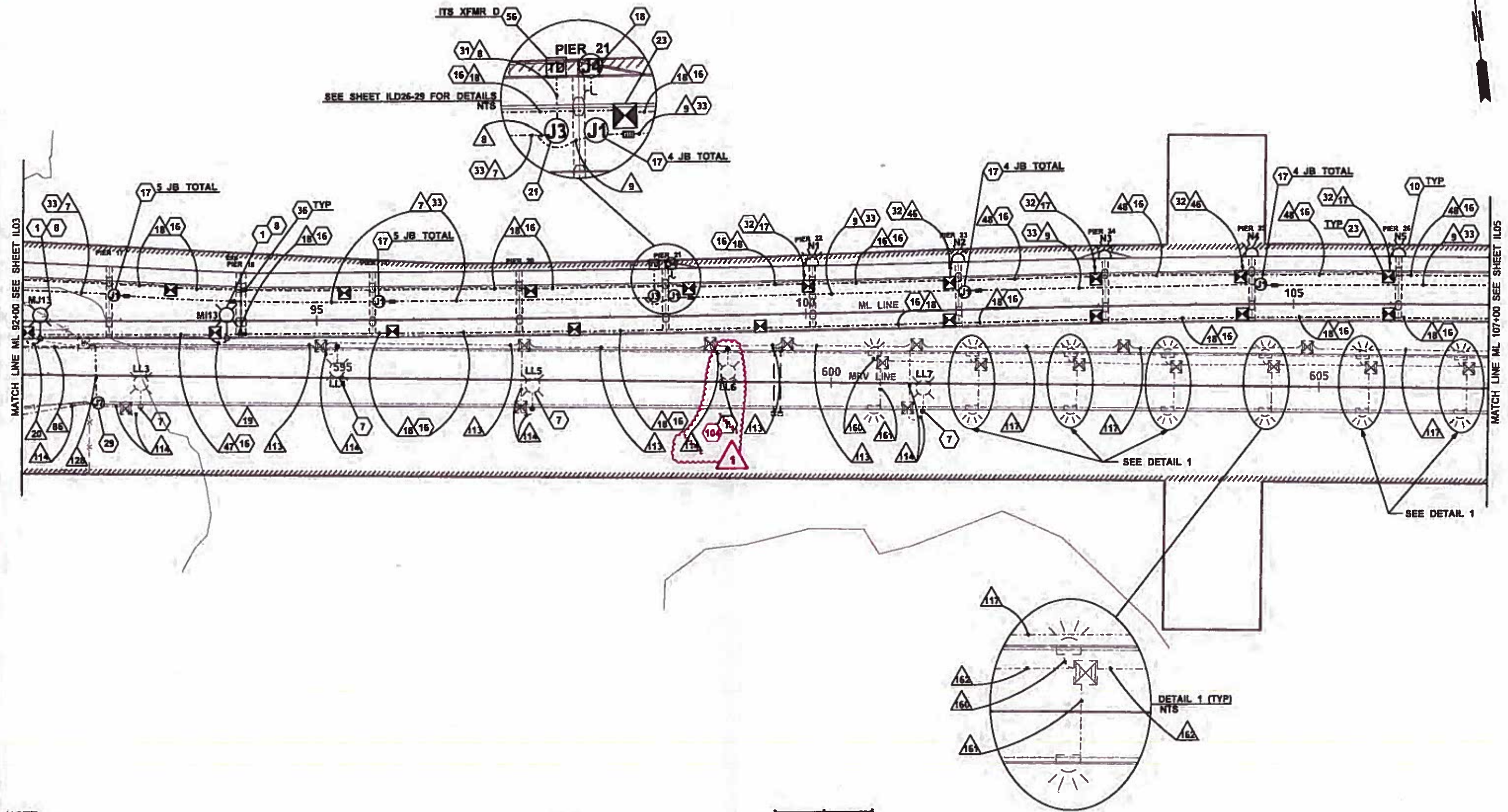
### NOTES:

1. SEE SHEET ILN01, ILN02 AND ILN05 FOR ABBREVIATIONS, GENERAL AND REMOVAL NOTES.
2. SEE ITS PLANS FOR REMOVAL OF EXISTING ITS TRANSFORMER.

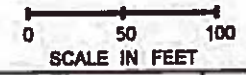
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REGION NO. 10		STATE WASH		JOB NUMBER 13A012		CONTRACT NO.		FED.AID PROJ.NO.		LOCATION NO.		DATE 10/27/15		BY KM		REVISION	
										Contract 8625 Change Order #72 Page 9 of 22		ILLUMINATION REMOVAL PLAN		PLAN REF. NO. ILR04  SHEET 582 OF 1797 SHEETS			



SEC. 22, T.25N. R.4E. W.M.



**NOTE:**  
SEE SHEET ILN01 - ILN05 FOR ILLUMINATION LEGEND, ABBREVIATIONS, GENERAL NOTES, CONSTRUCTION AND WIRE NOTES.



FILE NAME		REGION NO.		STATE		FED.AID PROJ.NO.					<div style="border: 1px solid red; padding: 5px; text-align: center;">                     Contract 8625                      Change Order #72                      Page 10 of 22                 </div>	PLAN REF. NO.
TIME	11:37:41 AM	10	WASH									591
DATE	6/2/2016	JOB NUMBER		13A012		LOCATION NO.						1797
PLOTTED BY	schlcv	CONTRACT NO.										SHEETS
DESIGNED BY	M. KUNDURU	DATE		10/27/15		BY						1797
ENTERED BY	M. KUNDURU	REVISION		CO#78 - MODS FROM DAMAGED SB								1797
CHECKED BY	J.YANG/D. SMS											1797
PROJ. ENGR.	D. EDWARDS											1797
REGIONAL ADM.	J. MEREDITH											1797

ILLUMINATION PLAN

**BREAKER SCHEDULE**      TYPE E SERVICE, 240/ 480V, NEAR LAKE WASHINGTON BLVD (SUA 5594)

CIRCUIT	DESCRIPTION	BREAKER RATING	BREAKER TYPE	CONTACTOR RATING	VOLTAGE (V)	LOAD (KVA)
-	MAIN	200	DPST	-	480	-
A	ILLUMINATION A (PEDESTRIAN UNDERDECK)	30	DPST	30	480	1.2
B	ILLUMINATION B (WB LOOP ON-RAMP, 24TH AVE UNDERDECK & MAINLINE)	30	DPST	30	480	5.2
C	ILLUMINATION C (NAVIGATION EB BRIDGE)	25	DPST	30	480	7.5
D	ILLUMINATION D (MAINLINE NEAR MONTLAKE)	30	DPST	30	480	3.9
E	ILLUMINATION E (WB LOOP ON-RAMP AND EASTBOUND-OFF RAMP)	30	DPST	30	480	2.6
F	ILLUMINATION F (EASTBOUND UNION BAY BRIDGE)	30	DPST	30	480	3.7
G	ILLUMINATION G (MONTLAKE UNDERDECK)	30	DPST	30	480	2.3
H	TRANSFORMER H (ITS)	60	DPST	N/A	480	20.0
I	TRANSFORMER I (ITS)	20	DPST	N/A	480	7.5
J	TRANSFORMER J (ITS)	20	DPST	N/A	480	7.5
K	TRANSFORMER K (ITS)	20	DPST	N/A	480	7.5
L	ILLUMINATION L (EASTBOUND FOSTER ISLAND)	30	DPST	30	480	2.4
M	INTERNAL TRANSFORMER M	15	DPST	N/A	480	3.0
N	SPARE	20	DPST	-	480	-
O	SPARE	20	DPST	-	480	-
PEAK LOAD (INCLUDING FUTURE LOAD)						84.8 74
CONTINUOUS LOAD						58.0 46

**BREAKER SCHEDULE**      TYPE E SERVICE, 240/ 480V, NEAR ARBORETUM (SUA 5596)

CIRCUIT	DESCRIPTION	BREAKER RATING	BREAKER TYPE	CONTACTOR RATING	VOLTAGE (V)	LOAD (KVA)
-	MAIN	70	DPST	-	480	-
A	TRANSFORMER A (ES/CCTV)	30	DPST	N/A	480	7.5
B	ILLUMINATION B (EASTBOUND ON-RAMP)	30	DPST	30	480	2.0
C	ILLUMINATION C (EASTBOUND ON-RAMP)	30	DPST	30	480	2.4
D	SPARE	20	DPST	N/A	480	-
E	SPARE	20	DPST	N/A	480	-
F	-	-	-	N/A	480	-
G	-	-	-	N/A	480	-
H	-	-	-	N/A	480	-
I	-	-	-	N/A	480	-
J	-	-	-	N/A	480	-
K	-	-	-	N/A	480	-
L	-	-	-	N/A	480	-
M	INTERNAL TRANSFORMER M	15	DPST	N/A	480	3.0
-	-	-	-	N/A	480	-
-	-	-	-	N/A	480	-
PEAK LOAD (INCLUDING FUTURE LOAD)						20.0
CONTINUOUS LOAD						5.1

**BREAKER SCHEDULE**      MODIFIED TYPE E SERVICE, 277/ 480V, NEAR CSTW-FACILITY M (SUA 5595)

CIRCUIT	DESCRIPTION	BREAKER RATING	BREAKER TYPE	CONTACTOR RATING	VOLTAGE (V)	LOAD (KVA)	PHASE (A, B, C)
-	MAIN	225	-	-	480/277	-	-
A	PEDESTRIAN LIGHTING (ABUTMENT LC1)	40	TPST	N/A	480	15.0	ABC
B	TRANSFORMER B (ITS NEAR ABUTMENT)	70	DPST	N/A	480	25.0	CA
C	TRANSFORMER C (ITS PIER 16)	40	DPST	N/A	480	15.0	BC
D	TRANSFORMER D (ITS PIER 21)	40	DPST	N/A	480	15.0	AB
E	TRANSFORMER E (NAV PIER 28)	25	DPST	N/A	480	7.5	CA
F	TRANSFORMER F (ITS PIER 28)	40	DPST	N/A	480	15.0	BC
G	TRANSFORMER G (ITS PIER 36)	40	DPST	N/A	480	15.0	AB
H	ILLUMINATION H (WB MAINLINE WEST OF ABUTMENT)	30	DPST	30	480	0.9	BC
I	ILLUMINATION I (WB MAINLINE/RAMP ON BRIDGE)	30	DPST	30	480	4.4	BC
J	ILLUMINATION J (WB MAINLINE/RAMP ON BRIDGE)	30	DPST	30	480	4.4	AB
K	ILLUMINATION K (WB MONTLAKE OFF RAMP WEST OF ABUTMENT)	30	DPST	30	480	3.6	BC
L	ILLUMINATION L (WB 24TH AVE E OFF RAMP WEST OF ABUTMENT)	30	DPST	30	480	2.6	BC
M	INTERNAL TRANSFORMER M	15	DPST	N/A	480	3.0	AB
N	PEDESTRIAN LIGHTING (PIER 16 (LC2), PIER 28 (LC3))	40	TPST	N/A	480	15.0	ABC
O	SPARE	20	DPST	-	480	-	-
P	SPARE	20	DPST	-	480	-	-
PEAK LOAD (INCLUDING FUTURE LOAD)*						150.0	
CONTINUOUS LOAD						86.1	

**BREAKER SCHEDULE**      NAVIGATION LIGHTING CONTROL CABINET LC4, 120/240V ON PIER 28

CIRCUIT	DESCRIPTION	BREAKER RATING	BREAKER TYPE	CONTACTOR RATING	VOLTAGE (V)	LOAD (KVA)
-	MAIN	60	-	-	120/240	-
A	PIER LIGHTING	30	SPST	30	120	0.2
B	PIER LIGHTING	30	SPST	30	120	0.2
C	RECEPTACLE	20	SPST	N/A	120	1.8
D	HEATER	15	SPST	N/A	120	0.1
E	LIGHTING CONTROL SWITCH	15	SPST	N/A	120	0.01
F	SPARE	20	DPST	N/A	240	-
G	SPARE	20	DPST	N/A	240	-
PEAK LOAD						2.3
CONTINUOUS LOAD						0.5

NAME: XFMR E (PIER 28)      SERVICE: SUA 5595 - CIRCUIT E  
 TYPE: 15 KVA 480V-120V/240V      BUSSWORK: RATED AT 200 AMP  
 CABINET: 60"(H) x 32"(W) x 30"(D)

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
E	PRIMARY MAIN	25	DPST	480V	-	7.5
EE	SECONDARY MAIN	45	DPST	240V	-	-
E-1	NAVIGATION CABINET	30	DPST	120/240V	10	2.4
E-2	-	-	-	-	-	-
E-3	SPARE	20	SPST	120V	-	-
E-4	SPARE	20	SPST	120V	-	-
E-5	-	-	-	-	-	-
E-6	RECEPTACLE	20	SPST	120V	15	1.8
TRANSFORMER RATED LOAD						15
TRANSFORMER ACTUAL LOAD						4.2

\* FIRST 10KVA RECEPTACLE LOAD AT 100% DEMAND FACTOR AND REMAINING OVER 10KVA AT 50% DEMAND FACTOR IS USED IN CALCULATING PEAK LOAD.

NOTE: SEE SHEET ILN01-02 FOR ILLUMINATION LEGEND, ABBREVIATIONS AND GENERAL NOTES.

FILE NAME	8:10:01 AM	TIME	6/6/2016	DATE	10 WASH	FED.AID PROJ.NO.	
PLOTTED BY	schlec	DESIGNED BY	M. KUNDURU	CHECKED BY	J. YANGD. SIMS	PROJ. ENGR.	D. EDWARDS
REGIONAL ADM.	J. MEREDITH	REVISION	CO#78 - MODS FROM DAMAGED SB AS-BUILT REVISIONS	DATE	10/27/15 3/13/15	BY	MK JC

6/7/16

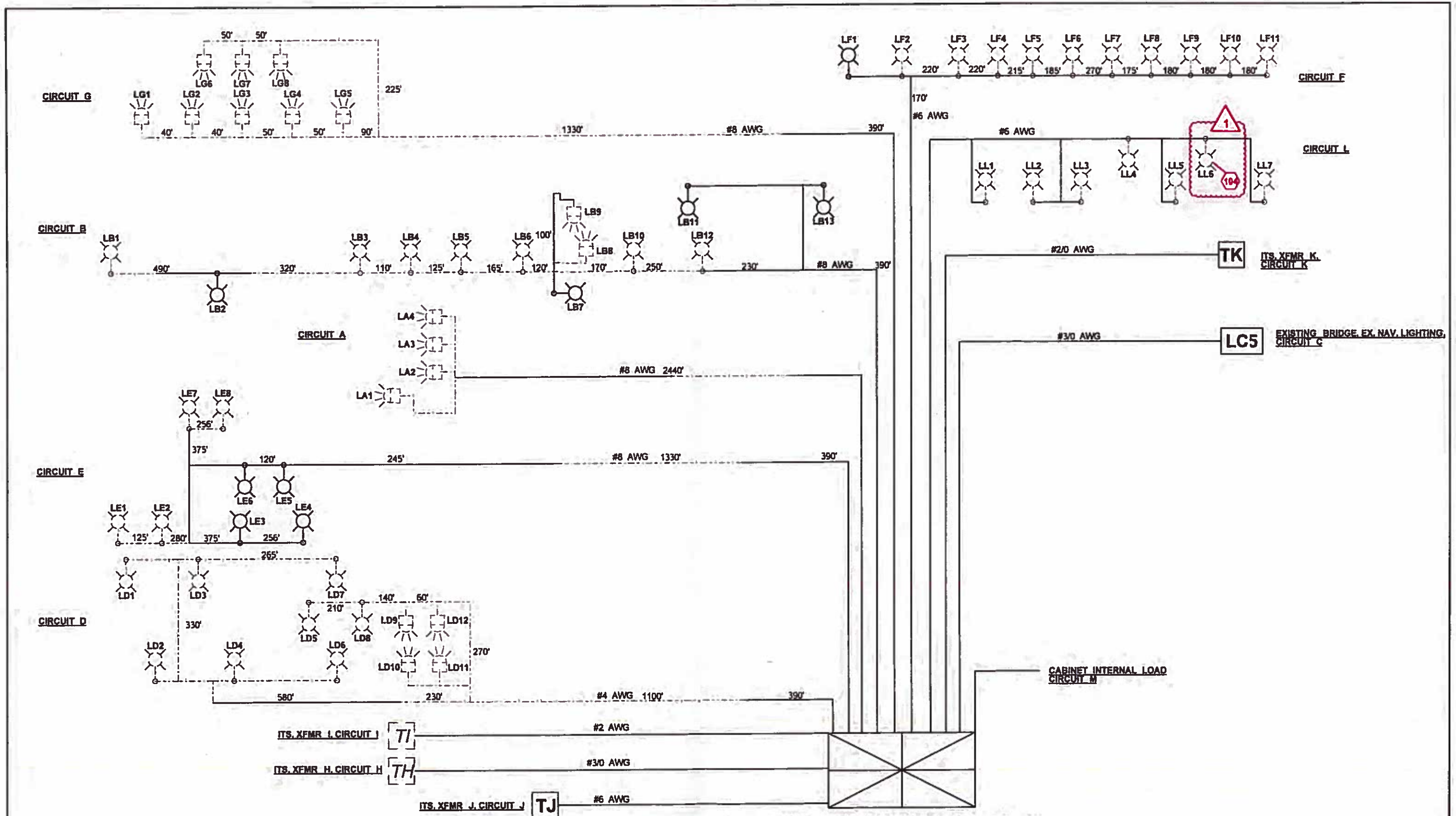
PARSONS BRINCKERHOFF

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PLAN REF. NO. ILD03

SHEET 599 OF 1797 SHEETS

**ILLUMINATION DETAILS**



**TYPE E WSDOT SERVICE CABINET  
NEAR LAKE WASHINGTON BLVD (LWB)**

**NOTE:**  
SEE SHEET ILN01-02 FOR ILLUMINATION LEGEND, ABBREVIATIONS AND GENERAL NOTES.

FILE NAME		REGION NO.		STATE		FED.AID PROJ.NO.					<div style="border: 1px solid red; padding: 5px; color: red; text-align: center;">           Contract 8625            Change Order #72            Page 12 of 22         </div>	PLAN REF. NO.
TIME	11:39:07 AM	10	WASH				ILD10					
DATE	6/2/2016	JOB NUMBER		13A012								SHEET
PLOTTED BY	schiccv	CONTRACT NO.				LOCATION NO.						606
DESIGNED BY	M. HUYNH/M. KUNDURU											OF
ENTERED BY	M. HUYNH/M. KUNDURU											1797
CHECKED BY	J. YANG/D. SIMS											SHEETS
PROJ. ENGR.	D. EDWARDS	REVISION		1028/15		DATE						
REGIONAL ADM.	J. MEREDITH	CO#78 - MODS FROM DAMAGED SB		KM		BY						

PARSONS BRINCKERHOFF Paramatrix

ILLUMINATION DETAILS

△ WIRE NOTES


RUN NO.	EXISTING CONDUIT	NEW CONDUIT	INNERDUCT	EXISTING CONDUCTOR	NEW CONDUCTOR	REMARK	
1	4" PVC		A	1-24SMFO*		MAINLINE COMMUNICATIONS	
			B	1-48SMFO		VIDEO/DATA DISTRIBUTION (PRIMARY)	
			C		1-96SMFO	MAINLINE COMMUNICATIONS	
			D	1-25TWP*	1-48SMFO	DATA COMMUNICATION, VIDEO/DATA DISTRIBUTION (SECONDARY)	
2	4" PVC		A	1-48SMFO		VIDEO/DATA DISTRIBUTION (PRIMARY)	
			B,C,D	EMPTY		SPARE	
3	4" PVC		A	1-24SMFO*		MAINLINE COMMUNICATIONS	
			B	EMPTY		SPARE	
			C		1-96SMFO	MAINLINE COMMUNICATIONS	
			D	1-25TWP*	1-48SMFO	DATA COMMUNICATION, VIDEO/DATA DISTRIBUTION (SECONDARY)	
4		2" RGS			6-2C (sh)	DETECTOR LEAD-IN	
5		2.5" RGS			9-#6	DMS POWER	
		2.5" RGS			EMPTY	SPARE	
6	4" PVC		A	1-48SMFO		VIDEO/DATA DISTRIBUTION (PRIMARY)	
			B,C,D	EMPTY		SPARE	
			A,B		EMPTY	SPARE	
			C	1-96SMFO		MAINLINE COMMUNICATIONS	
7	4" RGS		D	1-48SMFO		VIDEO/DATA DISTRIBUTION (SECONDARY)	
			A	1-48SMFO		VIDEO/DATA DISTRIBUTION (PRIMARY)	
			B	1-48SMFO		VIDEO/DATA DISTRIBUTION (BRIDGE CONTROL SYSTEM)	
			C,D	EMPTY		SPARE	
8	4" RGS		A	1-96SMFO		MAINLINE COMMUNICATIONS	
			B,C,D	EMPTY		SPARE	
8	2" RGS			1-CCC		CAMERA CABLE	
				1-WTC		WAVETRONICS CABLE (ALPHA 6023-C)	
9	4" RGS		A	1-48SMFO		VIDEO/DATA DISTRIBUTION (SECONDARY)	
			B,C,D	EMPTY		SPARE	
			A	1-96SMFO*		MAINLINE COMMUNICATIONS	
			B		1-96SMFO	MAINLINE COMMUNICATIONS	
10	4" RGS		C,D	EMPTY		SPARE	
			A		1-48SMFO	VIDEO/DATA DISTRIBUTION (PRIMARY)	
			B		1-48SMFO	VIDEO/DATA DISTRIBUTION (BRIDGE CONTROL SYSTEM)	
			C,D	EMPTY		SPARE	
10	4" RGS		A,B,C,D	EMPTY		SPARE	
			A	1-24SMFO*		MAINLINE COMMUNICATIONS	
11	4" RGS		B	1-48SMFO*		VIDEO/DATA DISTRIBUTION (SECONDARY)	
			C		1-48SMFO	VIDEO/DATA DISTRIBUTION (SECONDARY)	
			D	1-25TWP TEMP*		DATA COMMUNICATION	
					1-48SMFO	VIDEO/DATA DISTRIBUTION (LATERAL)	
12		2" RGS			2-2C (sh)	DETECTOR LEAD-IN	
13	2" RGS					1-48FOPP	CABINET COMMUNICATION
						EMPTY	SPARE
14		2" RGS			1-CAT6	CABINET COMMUNICATION	
15	2.5"			1-25TWP TEMP*			DATA COMMUNICATION
				1-25TWP*			DATA COMMUNICATION
16	2"			EMPTY			EMPTY
				1-25TWP*			DATA COMMUNICATION
17		3"				1-48FOPP*	CABINET COMMUNICATION
18	1.5" RGS**					2-48SMFO*	VIDEO/DATA DISTRIBUTION (SECONDARY)
19	2" RGS**					2-48SMFO*	VIDEO/DATA DISTRIBUTION (SECONDARY)

RUN NO.	EXISTING CONDUIT	NEW CONDUIT	INNERDUCT	EXISTING CONDUCTOR	NEW CONDUCTOR	REMARK	
20	4" RGS		A	1-96SMFO*		MAINLINE COMMUNICATIONS	
			B	1-48SMFO		VIDEO/DATA DISTRIBUTION (SECONDARY)	
			C,D	EMPTY		SPARE	
21	2" RGS**					1-24FOPP*	CABINET COMMUNICATION
22	4" RGS		A,B,C,D	EMPTY			SPARE
23	2"					1-24FOPP	CABINET COMMUNICATION, DMS COMMUNICATION (FIBER OPTIC CABLE)
24	4" RGS		A	1-48SMFO		VIDEO/DATA DISTRIBUTION (PRIMARY)	
			B,C,D	EMPTY		SPARE	
			A	1-96SMFO		MAINLINE COMMUNICATIONS	
			B	1-48SMFO		VIDEO/DATA DISTRIBUTION (SECONDARY)	
25	2" RGS					4-4TWP #18	DMS COMMUNICATION (SEE NOTE)
						EMPTY	SPARE
27	2" RGS					5-4TWP #18	DMS COMMUNICATION (SEE NOTE)
						EMPTY	SPARE
28	2.5"					4-1/0	DMS POWER
						1-2/0	DMS POWER
29	2.5" RGS					4-2/0	DMS POWER
						4-1/0	DMS POWER
30	2"					4-2/0	DMS POWER
						1-2/0	DMS POWER
31	2" RGS					3-#8	POWER FROM TRANSFORMER TO CABINET
						EMPTY	SPARE
32	3"					8-2C (sh)	DETECTOR LEAD-IN
						EMPTY	SPARE
33	2" RGS					EMPTY	FOR FUTURE PROJECT
						EMPTY	FOR FUTURE PROJECT
34							NOT USED
35	2"					1-3C	BUS FLASHING BEACON ON MAINLINE
						1-2C (sh)	DETECTOR LEAD-IN
36	2"					EMPTY	SPARE
						2-2C (sh)	DETECTOR LEAD-IN
37	2"					EMPTY	SPARE
						6-2C (sh)	DETECTOR LEAD-IN
38	2" RGS					1-3C	BUS FLASHING BEACON ON MAINLINE
						1-CCC	CAMERA CABLE
39	2" RGS					2-2C (sh)	DETECTOR LEAD-IN
						EMPTY	SPARE
40	2.5" RGS					2-2C (sh)	DETECTOR LEAD-IN
						EMPTY	SPARE
41	3" RGS					EMPTY	FOR FUTURE PROJECT
						EMPTY	FOR FUTURE PROJECT
						6-1/0	DMS POWER
						5-3/0	DMS POWER


\* REMOVE EXISTING CABLES FROM CONDUIT.  
 \*\* REMOVE EXISTING CONDUIT.

NOTE: "TWP" IS A PLACE HOLDER FOR DMS COMMUNICATION CABLES. FIBER OR CAT6 ARE THE OTHER ALTERNATIVES.

FILE NAME	TIME 11:40:08 AM	DATE 6/2/2016	PLOTTED BY schlecv	DESIGNED BY T. CHI	ENTERED BY T. CHI	CHECKED BY D. SIMS	PROJ. ENGR. D. EDWARDS	REGIONAL ADM. J. MEREDITH
REGION NO. 10	STATE WASH	FED.AID PROJ.NO.	JOB NUMBER 13A012	CONTRACT NO.	LOCATION NO.	REVISION	DATE 10/27/15	BY MK







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PLAN REF. NO. TSN06  
  
 SHEET 675 OF 1797 SHEETS

**ITS WIRING SCHEDULE**

△ WIRE NOTES

RUN NO.	EXISTING CONDUIT	NEW CONDUIT	INNERDUCT	EXISTING CONDUCTOR	NEW CONDUCTOR	REMARK
118	1.5"			6-2C (sh)*		DETECTOR LEAD-IN
119	1.5"			6-2C (sh)*		DETECTOR LEAD-IN
		1"		2-#8*		POWER TO BUS FLASHING BEACONS
120	1.5"			5-2C (sh)*		DETECTOR LEAD-IN
121	1"			2-#8*		POWER TO BUS FLASHING BEACONS
122	2"			2-#8*		POWER TO BUS FLASHING BEACONS
123						NOT USED
124	2" RGS			1-24FOPP*	5-FOC	CABINET COMMUNICATION, DMS COMMUNICATION (FIBER OPTIC CABLE)
125	2" RGS**			1-24FOPP*		CABINET COMMUNICATION
126	1.5"			2-#8*		POWER TO BUS BEACON
				2-#8*		POWER TO BUS FLASHING BEACONS
127	1.5"			2-#8*		POWER TO BUS BEACON CABINET
				2-2C (sh)*		DETECTOR LEAD-IN
128	1"			2-#8*		POWER TO BUS FLASHING BEACONS
	1.5"			2-#8*		POWER TO BUS BEACON
129	1"			2-#8*		POWER TO BUS FLASHING BEACONS
130	1"			2-2C (sh)*		DETECTOR LEAD-IN (BUS BEACON)
131	2"			6-2C (sh)*		DETECTOR LEAD-IN
	1.25"			1-CCC		CAMERA CABLE
132	4" RGS		A	1-96SMFO*		MAINLINE COMMUNICATIONS
			B	1-48SMFO		VIDEO/DATA DISTRIBUTION (SECONDARY)
			C,D	EMPTY		SPARE
			A,B,C,D	EMPTY		SPARE
4" RGS			1-96SMFO*		MAINLINE COMMUNICATIONS	
4" FLEX			1-48SMFO		VIDEO/DATA DISTRIBUTION (SECONDARY)	
4" FLEX			EMPTY		SPARE	
134	3" RGS			1-24SMFO TEMP*		MAINLINE COMMUNICATIONS (TEMPORARY)
				1-48SMFO TEMP*		DATA COMMUNICATION, VIDEO/DATA DISTRIBUTION (TEMPORARY)
				1-25 TWP TEMP*		DATA COMMUNICATION (TEMPORARY)
135	2" HDPE			1-2C (sh)		DETECTOR LEAD-IN
				EMPTY		SPARE
				4-2C (sh)		DETECTOR LEAD-IN
136	2" HDPE			EMPTY		SPARE
				4-2C (sh)		DETECTOR LEAD-IN
				2"		DETECTOR LEAD-IN
137	2"			5-FOC	DMS COMMUNICATION (FIBER OPTIC CABLE)	
138	2" RGS			1-3C	ADVANCE WARNING SIGN	
139	2" HDPE			3-FOC	DMS COMMUNICATION (FIBER OPTIC CABLE)	
140	2" RGS			2-FOC	DMS COMMUNICATION (FIBER OPTIC CABLE)	
141	2" RGS			3-#8	DMS POWER	
142	1.5" FLEX			1-FOC	DMS COMMUNICATION (FIBER OPTIC CABLE)	
143	2"			2-#8 <sup>1</sup>		DMS POWER
144	2"			1-4TWP#18 <sup>1</sup>		DMS COMMUNICATION
				1-WX CABLE <sup>1</sup>		WAVETRONICS DATA
145	3" RGS			4-1/0	DMS POWER	
146	3" RGS			3-3/0		DMS POWER
				2-1/0		DMS POWER
147	3" RGS			3-3/0		DMS POWER
				1-24FOPP*	5-FOC	CABINET COMMUNICATION, DMS COMMUNICATION (FIBER OPTIC CABLE)
					SPARE	
148	1" RGS			2-5C (sh)*	2-5C (sh)	RAMP METER SIGNAL HEAD
149	1" RGS			1-5C (sh)*	1-5C (sh)	RAMP METER SIGNAL HEAD

RUN NO.	EXISTING CONDUIT	NEW CONDUIT	INNERDUCT	EXISTING CONDUCTOR	NEW CONDUCTOR	REMARK
150				1-5C (sh)*	1-5C (sh)	RAMP METER SIGNAL HEAD
151		2" RGS			1-CCC	CAMERA CABLE
152		2"			4-#4	POWER FROM TRANSFORMER TO CABINET
153		2" RGS			4-#4	POWER FROM TRANSFORMER TO CABINET
		2" RGS			EMPTY	SPARE
154		1" RGS			2-2C (sh)	PREFORMED INDUCTION DETECTOR LEAD IN
		1" RGS			2-2C (sh)	PREFORMED INDUCTION DETECTOR LEAD IN
		1" RGS			2-2C (sh)	PREFORMED INDUCTION DETECTOR LEAD IN
		1" RGS			2-2C (sh)	PREFORMED INDUCTION DETECTOR LEAD IN
155						NOT USED
156		2.5" RGS			EMPTY	FOR FUTURE PROJECT
		2.5" RGS			8-2C (sh)	PREFORMED INDUCTION DETECTOR LEAD IN
157		2" RGS			8-2C (sh)	PREFORMED INDUCTION DETECTOR LEAD IN
158		2" RGS			1-24FOPP	CABINET COMMUNICATION
		2" RGS			EMPTY	SPARE

\* REMOVE EXISTING CABLES FROM CONDUIT.  
 \*\* REMOVE EXISTING CONDUIT.

NOTE: "TWP" IS A PLACE HOLDER FOR DMS COMMUNICATION CABLES. FIBER OR CAT6 ARE THE OTHER ALTERNATIVES.

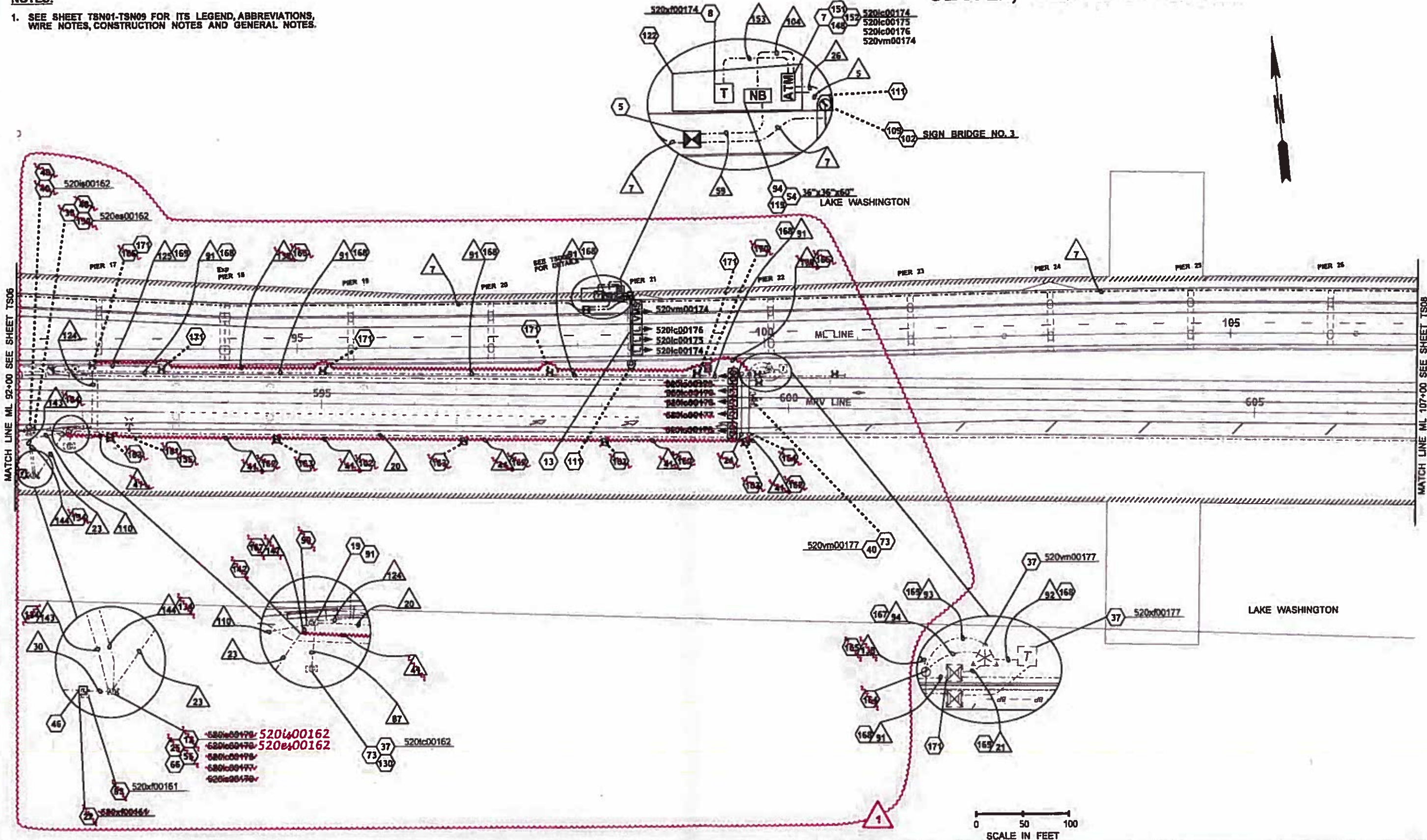
FILE NAME	TIME 11:40:34 AM	DATE 6/2/2016	DESIGNED BY T. CHI	ENTERED BY T. CHI	CHECKED BY D. SIMS	PROJ. ENGR. D. EDWARDS	REGIONAL ADM. J. MEREDITH
REVISION	CO#78 - MODS FROM DAMAGED SB	10/27/15	KM	CO#93 - ADDED LOOPS & CCTV MOD	01/07/16	JA	
DATE							

			<p>Contract 8625          Change Order #72          Page 14 of 22</p>	<p>PLAN REF. NO.          TSN09</p>
<p>PARSONS BRINCKERHOFF Parametrix</p>			<p>ITS WIRING SCHEDULE</p>	<p>SHEET 678 OF 1797 SHEETS</p>

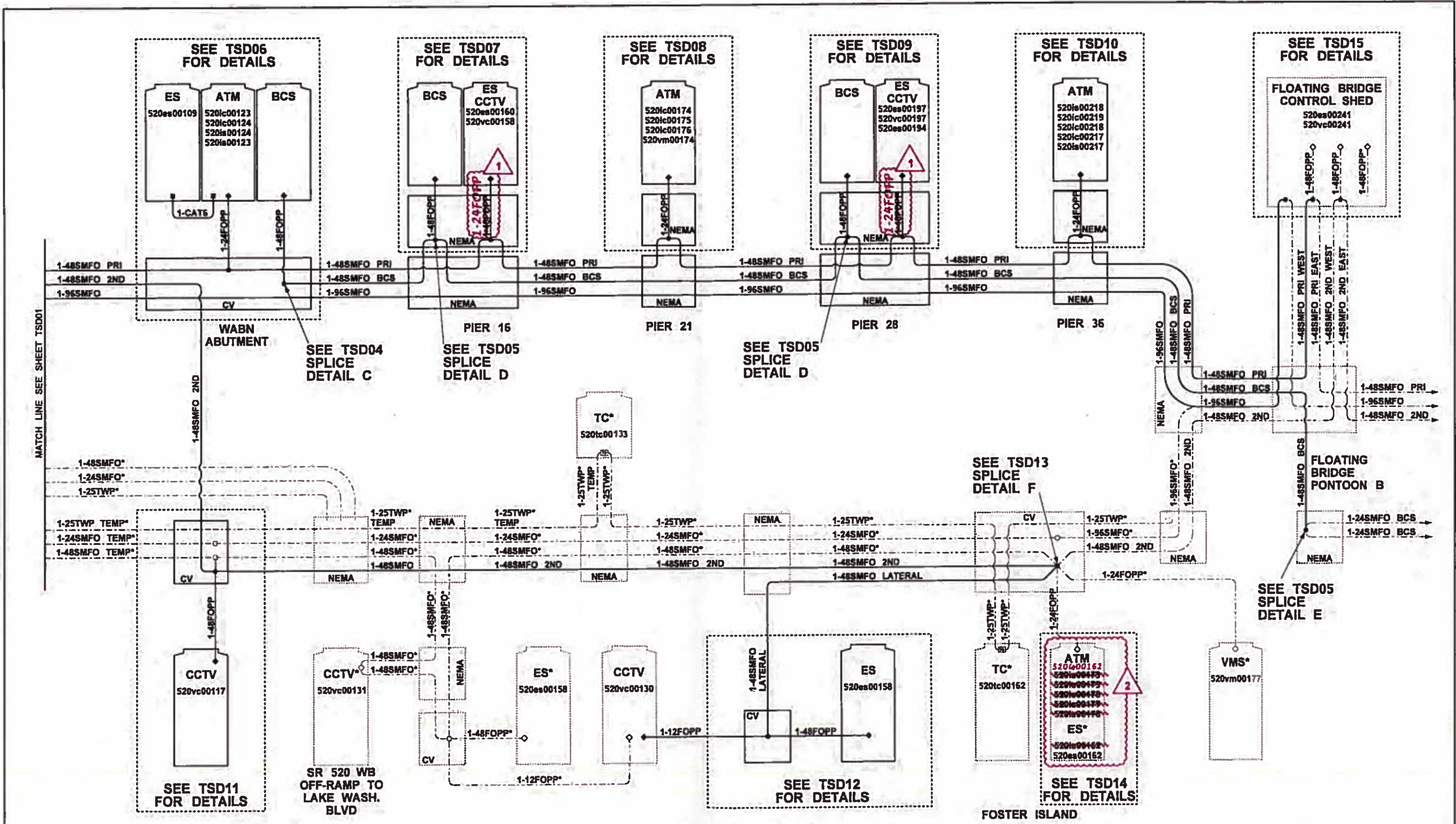
SEC. 22, T. 25 N. R. 4 E. W.M.

NOTES:

1. SEE SHEET TSN01-TSN09 FOR ITS LEGEND, ABBREVIATIONS, WIRE NOTES, CONSTRUCTION NOTES AND GENERAL NOTES.

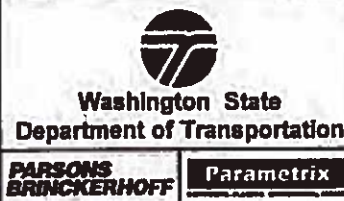


FILE NAME		REGION NO.		STATE		FED.AID PROJ.NO.				Washington State Department of Transportation		<div style="border: 1px solid red; padding: 5px; text-align: center;">                 Contract 8625                  Change Order #72                  Page 15 of 22             </div>	PLAN REF. NO.	
TIME	11:41:19 AM	10	WASH							PARSONS BRINCKERHOFF	Parametrix		ITS PLAN	685 OF 1797 SHEETS
DATE	6/2/2016	JOB NUMBER		13A012		CONTRACT NO.								
PLOTTED BY schlec		CONTRACT NO.		LOCATION NO.				DATE		6/7/16				
DESIGNED BY T. CHI		REVISION		CO#78 - MODS FROM DAMAGED SB		DATE		BY		10/27/15		MK		
ENTERED BY T. CHI														
CHECKED BY D. SIMS														
PROJ. ENGR. D. EDWARDS														
REGIONAL ADM. J. MEREDITH														



ITS DISTRIBUTION DIAGRAM

FILE NAME	TIME	DATE	DESIGNED BY	ENTERED BY	CHECKED BY	PROJ. ENGR.	REGIONAL ADM.	REVISION	DATE	BY	REGION NO.	STATE	FED.AID PROJ.NO.	LOCATION NO.	CONTRACT NO.	DATE	PLANNING	DESIGN	CONSTRUCTION	OPERATION	MAINTENANCE	PLANNING	DESIGN	CONSTRUCTION	OPERATION	MAINTENANCE
	4:01:51 PM	6/2/2016	echlccv	T. CHI	D. SIMS	D. EDWARDS	J. MEREDITH	CO#78 - MODS FROM DAMAGED SB	10/27/15	KM	10	WASH			13A012	6/7/16										
								CO#93 - ADDED LOOPS & CCTV MOD	01/07/16	JA																



Contract 8625  
Change Order #72  
Page 16 of 22

PLAN REF. NO.  
TSD02  
SHEET  
691  
OF  
1797  
SHEETS

ITS DETAILS

NAME: 520x00130 (LAKE WASH. BLVD)  
 TYPE: 7.5 KVA 480V-120V  
 CABINET: 48"(H) x 24"(W) x 20"(D)  
 SERVICE: SUA 5596 - CIRCUIT A  
 BUSSWORK: RATED AT 200 AMP

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
A	PRIMARY MAIN	20	DPST	480V	---	---
AA	SECONDARY MAIN	40	DPST	240V	---	---
A-1	520vc00130	20	SPST	120V	17.7	2.124
A-2	520es00158	20	SPST	120V	17.1	2.052
A-3	SPARE	-	-	-	-	-
A-4	SPARE	-	-	-	-	-
A-5	SPARE	-	-	-	-	-
A-6	GFCI RECEPTACLE	20	SPST	120V	15	1.8
TRANSFORMER RATED LOAD						7.50 KVA
TRANSFORMER ACTUAL LOAD						5.98 KVA

NAME: 520x00116 (CSTW-FACILITY M)  
 TYPE: 37.5 KVA 480V-120V/240V  
 CABINET: 60"(H) x 32"(W) x 30"(D)  
 SERVICE: SUA 5595 - CIRCUIT B  
 BUSSWORK: RATED AT 225 AMP

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
B	PRIMARY MAIN	100	DPST	480V	---	---
BB	SECONDARY MAIN	200	DPST	240V	---	---
	520ls00123 L1				L1=50.75	L1=6.09
	520ls00124 L2					
	520lc00123 L1					
	520lc00124 L2					
B-1	ATM CABINET	70	DPST	240V	L2=31.25	L2=3.75
B-3	520es00109	20	SPST	120V	17.10	2.052
B-4	FIRE PROTECTION CABINET	70	SPST	120V	54.167	6.5
B-5	BUS BEACON	20	SPST	120V	17.10	2.052
B-6	GFCI RECEPTACLE	20	SPST	120V	15.00	1.8
B-7	BCS	35	SPST	120V	29.167	3.5
TRANSFORMER RATED LOAD						37.50 KVA
TRANSFORMER ACTUAL LOAD						25.74 KVA

NAME: 520x00160 (PIER 16)  
 TYPE: 15 KVA 480V-120V/240V  
 CABINET: 60"(H) x 32"(W) x 30"(D)  
 SERVICE: SUA 5595 - CIRCUIT C  
 BUSSWORK: RATED AT 200 AMP

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
C	PRIMARY MAIN	40	DPST	480V	---	---
CC	SECONDARY MAIN	80	DPST	240V	---	---
C-1	520VC00158/520ES00160	25	SPST	120V	19.80	2.376
C-2	BCS	35	SPST	120V	29.167	3.5
C-3	SPARE	-	-	-	-	-
C-4	SPARE	-	-	-	-	-
C-5	SPARE	-	-	-	-	-
C-6	GFCI RECEPTACLE	20	SPST	120V	15.00	1.8
TRANSFORMER RATED LOAD						15.00 KVA
TRANSFORMER ACTUAL LOAD						7.68 KVA

NAME: 520x00174 (PIER 21)  
 TYPE: 15 KVA 480V-120V/240V  
 CABINET: 60"(H) x 32"(W) x 30"(D)  
 SERVICE: SUA 5595 - CIRCUIT D  
 BUSSWORK: RATED AT 200 AMP

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
D	PRIMARY MAIN	40	DPST	480V	---	---
DD	SECONDARY MAIN	80	DPST	240V	---	---
	520vm00174 L1-2				L1=38.19	L1=4.58
	520lc00174 L1					
	520lc00175 L2					
D-1	520lc00176 L1					
D-2	ATM CABINET	60	DPST	240V	L2=45.10	L2=5.41
D-3	SPARE	-	-	-	-	-
D-4	SPARE	-	-	-	-	-
D-5	SPARE	-	-	-	-	-
D-6	GFCI RECEPTACLE	20	SPST	120V	15.00	1.80
TRANSFORMER RATED LOAD						15.00 KVA
TRANSFORMER ACTUAL LOAD						11.80 KVA

NAME: 520x00194 (PIER 28)  
 TYPE: 15 KVA 480V-120V/240V  
 CABINET: 60"(H) x 32"(W) x 30"(D)  
 SERVICE: SUA 5595 - CIRCUIT F  
 BUSSWORK: RATED AT 200 AMP

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
F	PRIMARY MAIN	40	DPST	480V	---	---
FF	SECONDARY MAIN	80	DPST	240V	---	---
F-1	520VC00197/520ES00197	25	SPST	120V	19.80	2.376
F-2	BCS	35	SPST	120V	29.167	3.5
F-3	SPARE	-	-	-	-	-
F-4	SPARE	-	-	-	-	-
F-5	SPARE	-	-	-	-	-
F-6	GFCI RECEPTACLE	20	SPST	120V	15.00	1.8
TRANSFORMER RATED LOAD						15.00 KVA
TRANSFORMER ACTUAL LOAD						7.676 KVA

NAME: 520x00217 (PIER 36)  
 TYPE: 15 KVA 480V-120V/240V  
 CABINET: 60"(H) x 32"(W) x 30"(D)  
 SERVICE: SUA 5595 - CIRCUIT G  
 BUSSWORK: RATED AT 200 AMP

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
G	PRIMARY MAIN	40	DPST	480V	---	---
GG	SECONDARY MAIN	80	DPST	240V	---	---
	520ls00218 L1				L1=43.83	L1=5.26
	520ls00217 L2					
	520lc00219 L1					
	520lc00217 L2					
G-1	520lc00218 L1					
G-2	ATM CABINET	60	DPST	240V	L2=50.75	L2=6.09
G-3	SPARE	-	-	-	-	-
G-4	SPARE	-	-	-	-	-
G-5	SPARE	-	-	-	-	-
G-6	GFCI RECEPTACLE	20	SPST	120V	15.00	1.80
TRANSFORMER RATED LOAD						15.00 KVA
TRANSFORMER ACTUAL LOAD						13.15 KVA

NAME: 520x00161 (FOSTER ISLAND)  
 TYPE: 15 KVA 480V-120V/240V (UPGRADE) - 7.5 KVA 480V-120V/240V (EXISTING)  
 CABINET: 60"(H) x 24"(W) x 20"(D)  
 SERVICE: SUA 5594 - CIRCUIT K  
 BUSSWORK: RATED AT 200 AMP MINIMUM

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
K	PRIMARY MAIN	40	DPST	480V	---	---
KK	SECONDARY MAIN	80	DPST	240V	---	---
	520ls00178 L1				29.5	3.4
	520ls00178 L2					
	520lc00178 L1					
	520lc00178 L2					
K-1	520lc00177 L1				1.3	0.2
K-2	ATM CABINET	60	DPST	240V	L2=50.75	L2=6.09
K-3	SPARE	-	-	-	-	-
K-4	SPARE	-	-	-	-	-
K-5	SPARE	-	-	-	-	-
K-6	GFCI RECEPTACLE	20	SPST	120V	15.00	1.80
TRANSFORMER RATED LOAD						7.5 KVA
TRANSFORMER ACTUAL LOAD						5.4 KVA



NAME: 520x00117 (SR 520 EASTBOUND MONTLAKE)  
 TYPE: 7.5 KVA 480V-120V  
 CABINET: 48"(H) x 24"(W) x 20"(D)  
 SERVICE: SUA 5594 - CIRCUIT K  
 BUSSWORK: RATED AT 200 AMP

CIRCUIT NUMBER	DESCRIPTION	BREAKER RATING	BREAKER TYPE	VOLTAGE	AMP LOAD	KVA LOAD
J	PRIMARY MAIN	20	DPST	480V	---	---
JJ	SECONDARY MAIN	40	DPST	240V	---	---
J-1	520vc00117	20	SPST	120V	17.70	2.124
J-2	SPARE	-	-	-	-	-
J-3	SPARE	-	-	-	-	-
J-4	SPARE	-	-	-	-	-
J-5	SPARE	-	-	-	-	-
J-6	GFCI RECEPTACLE	20	SPST	120V	15.00	1.80
TRANSFORMER RATED LOAD						7.50 KVA
TRANSFORMER ACTUAL LOAD						3.924 KVA

**ABBREVIATIONS**

- DPST DOUBLE POLE SINGLE THROW
- SPST SINGLE POLE SINGLE THROW
- es ELECTRONIC SURVEILLANCE (DATA STATION / RAMP METERING)
- FB FLASHING BEACON (BUSES ENTERING HIGHWAY WARNING SIGN)
- FTC FIBER TERMINAL CABINET
- GFCI GROUND FAULT CIRCUIT INTERRUPTER
- ls INFORMATION AND SPEED SIGN / SIDE-MOUNTED MESSAGE SIGN
- lc LANE CONTROL SIGN
- vc CLOSED CIRCUIT TELEVISION VIDEO CAMERA
- vm VARIABLE MESSAGE SIGN

**TRANSFORMER BREAKER SCHEDULE**

FILE NAME	TIME 11:45:45 AM	DATE 6/2/2016	REGION NO. 10	STATE WASH	FED.AID PROJ.NO.	 	Contract 8625 Change Order #72 Page 17 of 22	PLAN REF. NO. TSD16
PLOTTED BY schicv	DESIGNED BY K. MANMITH	ENTERED BY K. MANMITH	JOB NUMBER 13A012	CONTRACT NO.	LOCATION NO.			SHEET 705 OF 1797 SHEETS
PROJ. ENGR. D. EDWARDS	REGIONAL ADM. J. MEREDITH	REVISION	DATE 10/27/15	BY KM	DATE 6/7/16	PARSONS BRINCKERHOFF	Paramatrix	ITS DETAILS



**GENERAL NOTES**

- CABINET SHALL BE FABRICATED OF MILL FINISHED ALUMINUM. CABINET SHALL NOT BE PAINTED OR ANODIZED ALUMINUM.
- THE SECONDARY BRANCH BREAKERS SHALL BE BOLT IN TYPE, MOUNTED ON COPPER BUSSWORK RATED AT 100 AMPS.
- ALL CIRCUIT BREAKERS INSTALLED SHALL BE PERMANENTLY MARKED WITH APPLICABLE CIRCUIT NAME.
- THE DEAD FRONT SHALL BE FABRICATED IN TWO PARTS, ONE FOR EACH COMPARTMENT.
- THE DOOR HINGE SHALL BE MOUNTED ON THE LEFT SIDE. CONSTRUCTION CORES SHALL BE GREEN WITH TWO MASTER KEYS AND ONE CORE KEY PER LOCK.
- THE ANCHOR BOLT PATTERN SHALL BE AS SPECIFIED BY THE CABINET MANUFACTURER. CABINET ANCHOR BOLTS SHALL EITHER CONFORM TO SECTION 9-06.5(1) AND BE GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH AASHTO M 232, OR BE STAINLESS STEEL. THE THREADED END OF CABINET ANCHOR BOLTS SHALL EXTEND A MINIMUM OF 1-1/2 INCHES ABOVE THE TOP SURFACE OF THE CONCRETE PAD FOUNDATION.
- SEE SECTION 9-29.25 FOR ADDITIONAL REQUIREMENTS.
- TRANSFORMER VOLTAGE AND KVA REQUIREMENTS ARE AS SHOWN IN THE ITS PLANS AND SPECIAL PROVISIONS. TRANSFORMERS 7.5 KVA AND LARGER SHALL BE SUPPLIED WITH TWO FULL CAPACITY TAPS, ONE AT 5% AND ONE AT 10% BELOW NORMAL CAPACITY.
- SEE TRANSFORMER BREAKER SCHEDULE FOR SIZE AND QUANTITY OF BREAKERS.
- SEE CABINET FOUNDATION DETAILS FOR CABINET AND CONDUIT DETAILS.

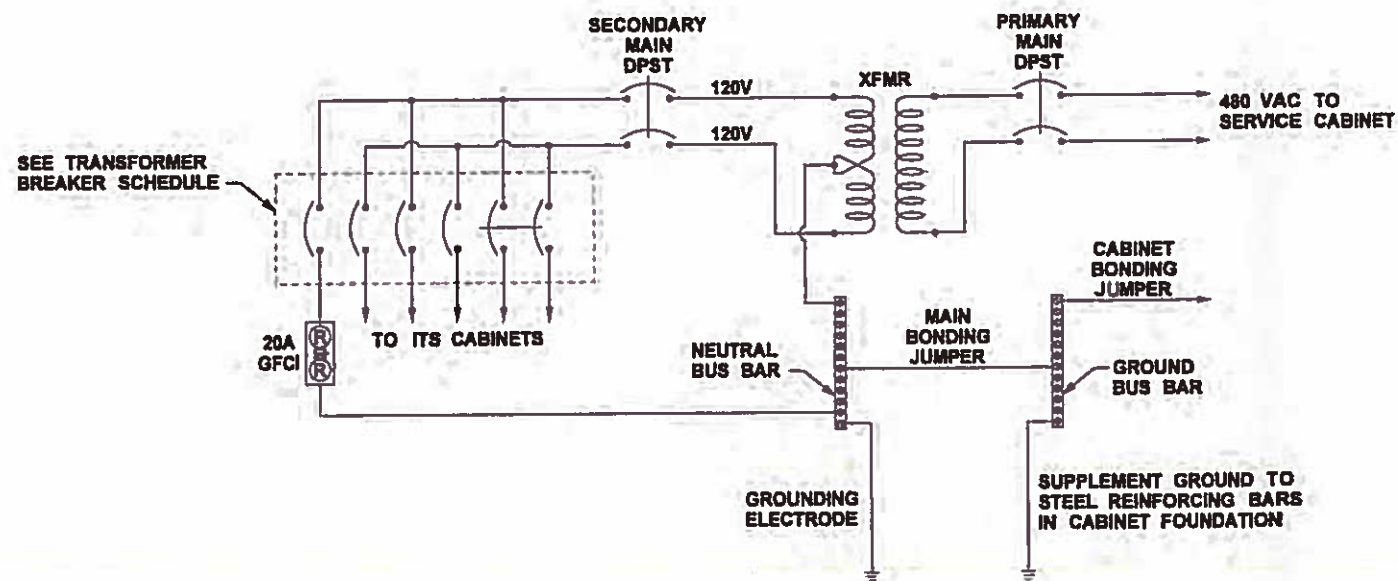
**LEGEND**

- EQUIPMENT GROUND
- DUPLEX RECEPTACLE WITH GFCI
- SPST CIRCUIT BREAKER
- DPST CIRCUIT BREAKER

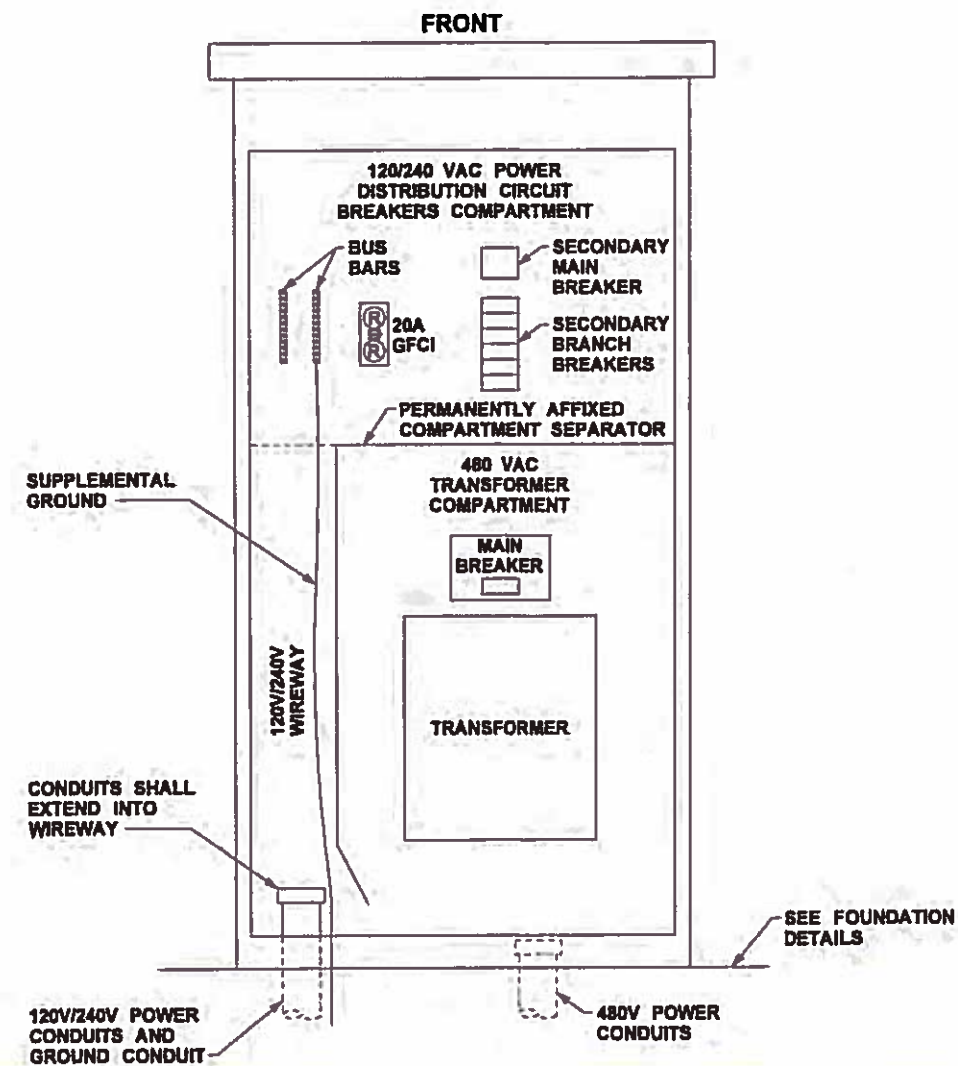
**ABBREVIATIONS**

- DPST DOUBLE POLE SINGLE THROW
- SPST SINGLE POLE SINGLE THROW
- GFCI GROUND FAULT CIRCUIT INTERRUPTER
- XFMR TRANSFORMER

TRANSFORMER CABINET DIMENSIONS				
NAME	RATING	HEIGHT	WIDTH	DEPTH
520x00117	7.5 KVA	48"	24"	20"
520x00130	7.5 KVA	48"	24"	20"
520x00116	37.5 KVA	60"	32"	30"
520x00160	15 KVA	60"	32"	30"
520x00174	15 KVA	60"	32"	30"
520x00194	15 KVA	60"	32"	30"
520x00217	15 KVA	60"	32"	30"
520x00461	15 KVA	60"	32"	20"



**TRANSFORMER CABINET ONE-LINE POWER DIAGRAM**  
480V - 120V/240V

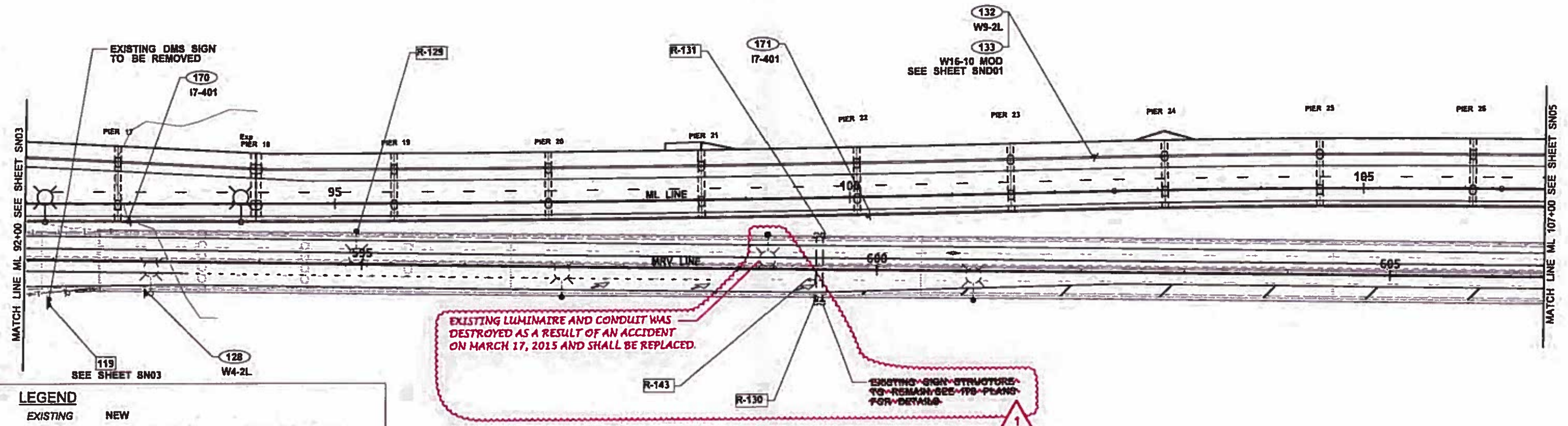


**TRANSFORMER CABINET DETAILS**  
WITH DEAD FRONTS REMOVED

FILE NAME	TIME 11:46:59 AM	DATE 6/2/2016	PLOTTED BY echicv	DESIGNED BY T. CHI	ENTERED BY T. CHI	CHECKED BY D. SIMS	PROJ. ENGR. D. EDWARDS	REGIONAL ADM. J. MEREDITH
REVISION	CO#78 - MODS FROM DAMAGED SB	10/27/15	KM	DATE	BY			
REGION NO.	STATE	FED.AID PROJ.NO.	10	WASH	JOB NUMBER	13A012	CONTRACT NO.	LOCATION NO.

					Contract 8625 Change Order #72 Page 18 of 22	PLAN REF. NO. TSD17
					ITS DETAILS	SHEET 706 OF 1797 SHEETS

SEC.22 T.25N. R.4E. W.M.



**LEGEND**

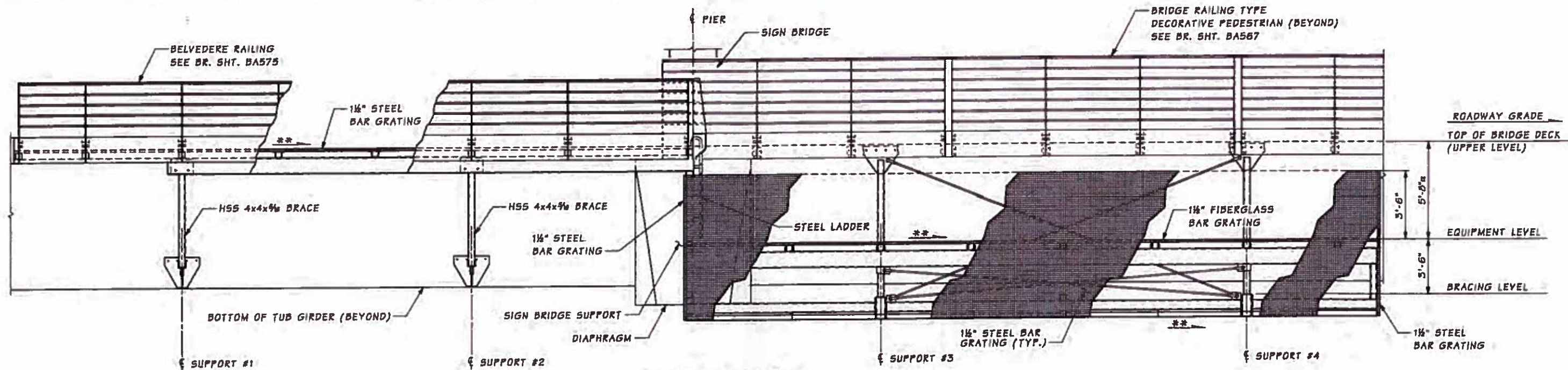
EXISTING	NEW	
		TRUSS SIGN BRIDGE STRUCTURE
		SIGN WITH SINGLE POST
		SIGN WITH DOUBLE POSTS
		SIGNAL POLE
		OVERHEAD SIGN
		MONOTUBE SIGN BRIDGE STRUCTURE
		NEW SIGN NOTE
		SIGN REMOVAL NOTE
		LIGHT STANDARD
		(EXISTING LOCATION) SIGN RELOCATION NOTE (NEW LOCATION)



**NOTES**

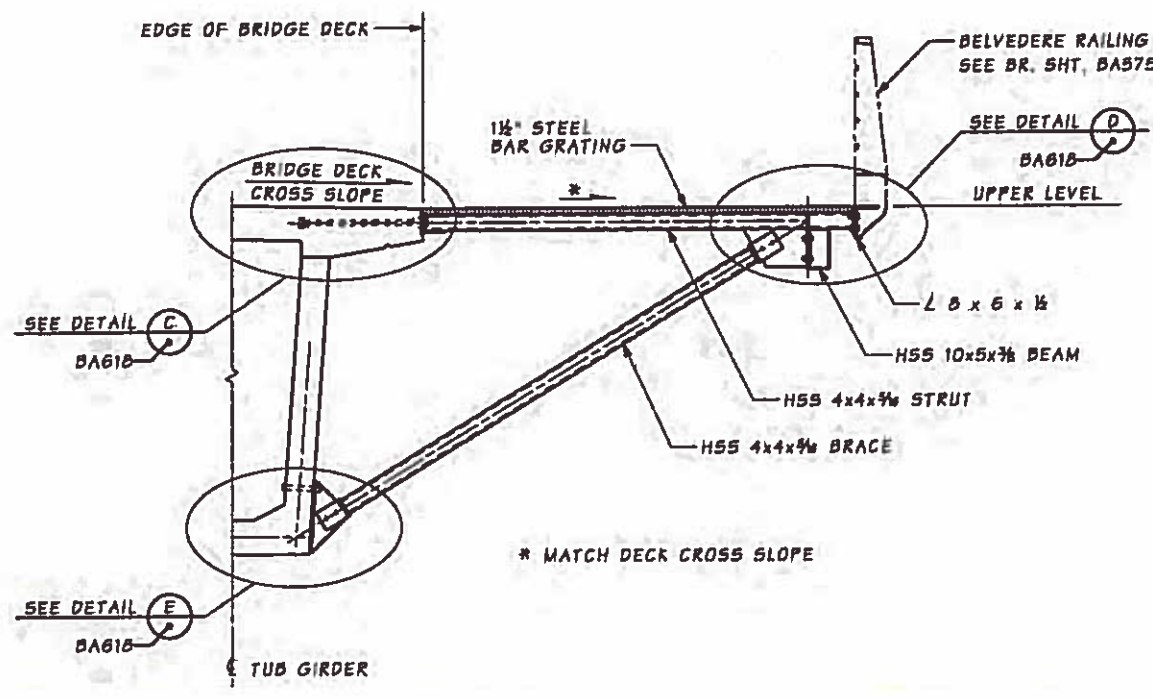
- UNLESS NOTED OTHERWISE, EXISTING SIGNS SHALL REMAIN IN SERVICE AND BE PROTECTED FROM CONSTRUCTION ACTIVITIES. EXISTING MAINLINE GUIDE SIGNS SHALL REMAIN IN PLACE UNTIL REMOVAL OF EXISTING SIGN STRUCTURES AND BRIDGE STRUCTURES.
- ANY SIGN REMOVAL SHALL BE REPLACED WITH NEW SIGN WITH SIMILAR MESSAGING IF ROADWAY CONDITIONS STILL APPLY. NEW SIGNS SHALL BE DESIGNED IN ACCORDANCE WITH WSDOT DESIGN MANUAL, WSDOT SIGN FABRICATION MANUAL, NWR CURRENT SIGNING PRACTICES AND THE MUTCD. ANY CONFLICTING EXISTING SIGNS SHALL BE REMOVED AND OR REPLACED.
- SEE STRUCTURE AND ITS PLANS FOR SIGN STRUCTURE AND ATM SIGN DETAILS.
- SEE SIGNAL PLANS FOR ADDITIONAL OVERHEAD SIGNING AT SIGNALIZED INTERSECTIONS.

FILE NAME	TIME 11:47:59 AM	DATE 6/2/2016	PLOTTED BY schlecv	DESIGNED BY M. HUYNH	ENTERED BY M. HUYNH	CHECKED BY D. SIMS	PROJ. ENGR. D. EDWARDS	REGIONAL ADM. J. MEREDITH	REVISION	DATE	BY	REGION NO. 10	STATE WASH	FED.AID PROJ.NO.	LOCATION NO.						Contract 8625 Change Order #72 Page 19 of 22	PLAN REF. NO. SN04	SHEET 793 OF 1797 SHEETS
CO78 - MODS FROM DAMAGED SB										10/27/15	KM												
SIGN PLAN																							

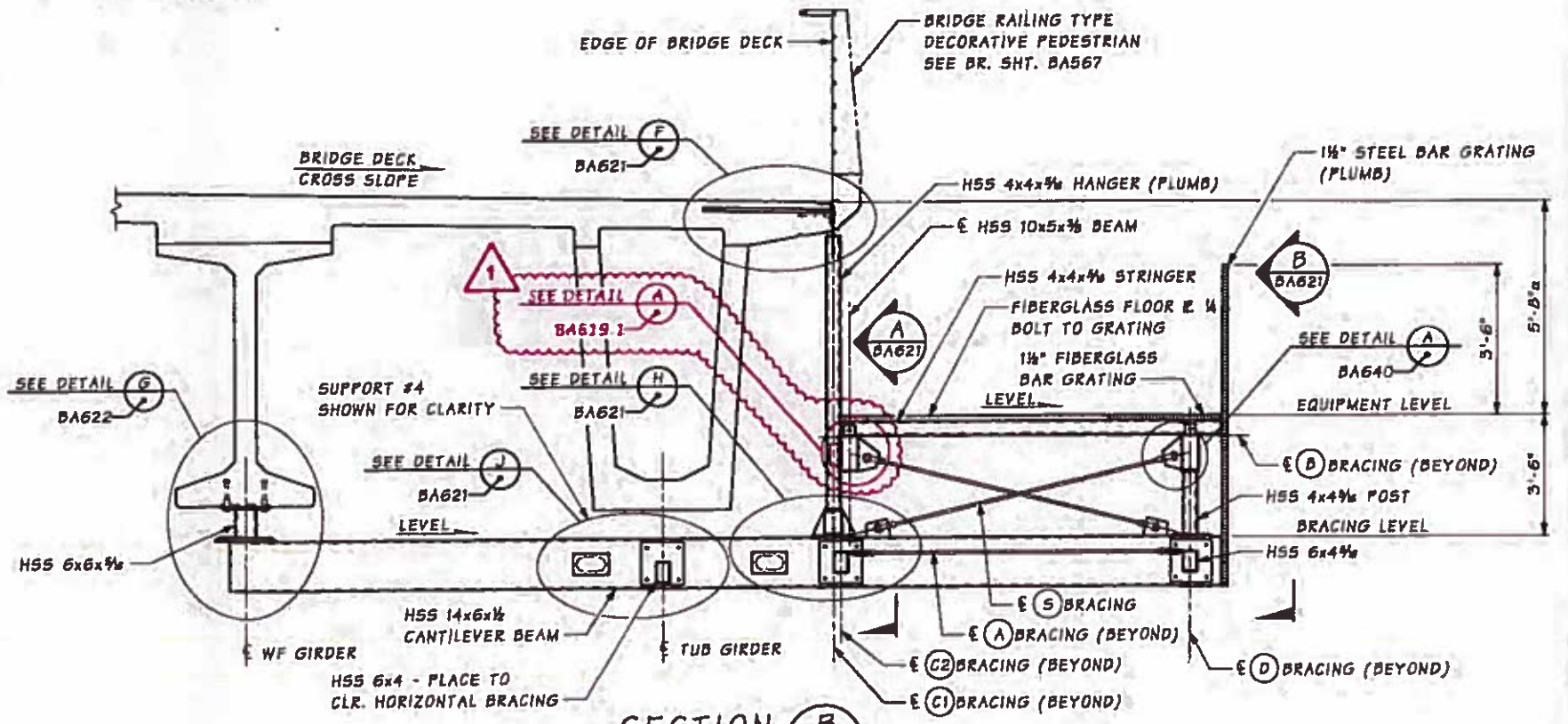


DEVELOPED ELEVATION  
ELEVATION ALONG OUTER EDGE  
EQUIPMENT PLATFORM BELVEDERE SHOWN  
BELVEDERE SIMILAR.

α DIMENSIONS TAKEN AT EDGE OF BRIDGE DECK



SECTION A  
SUPPORT #2 SHOWN  
(SUPPORT #1 AND  
STRINGERS SIMILAR)  
BA614 & BA615

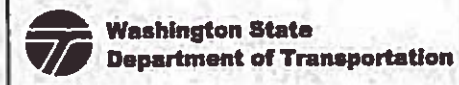
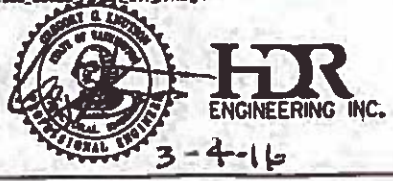


SECTION B  
BA616

α DIMENSIONS TAKEN AT EDGE OF BRIDGE DECK

SR 520 FILE NO. 7051 SHEET BA617 Projectwise Vault

Bridge Design Engr.	PW:ProgramDesign-Bld-Build/Contract_068628_WASH02.0_Contract Documents/2.05_Contract Plans/2.05.02_Ae-bld/Bld-Ae-Bldr DGN\W011553_CS825_P2_BA_617_R1.dgn						
Supervisor	BOTT, P.	REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
Designed By	KORPI, D.	10	WASH		1553	1797	
Checked By	KNUTSON, G.	JOB NUMBER					
Detailled By	AUDAR, M.	13AD12					
Bridge Projects Engr.							
Prelim. Plan By	12/04/16	CO#61 - EQUIP PLATFORM MOD	AS	GGK			
Architect/Specialist	DATE	REVISION	BY	APPD			

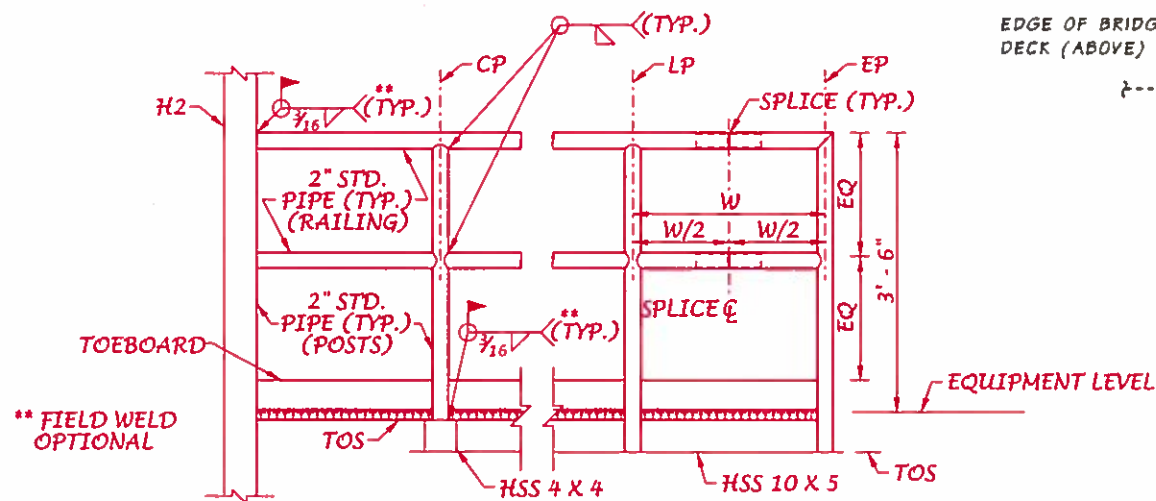


Contract 8625  
Change Order #72  
Page 20 of 22

BELVEDERE AND EQUIPMENT PLATFORM  
BELVEDERE ELEVATION & SECTIONS

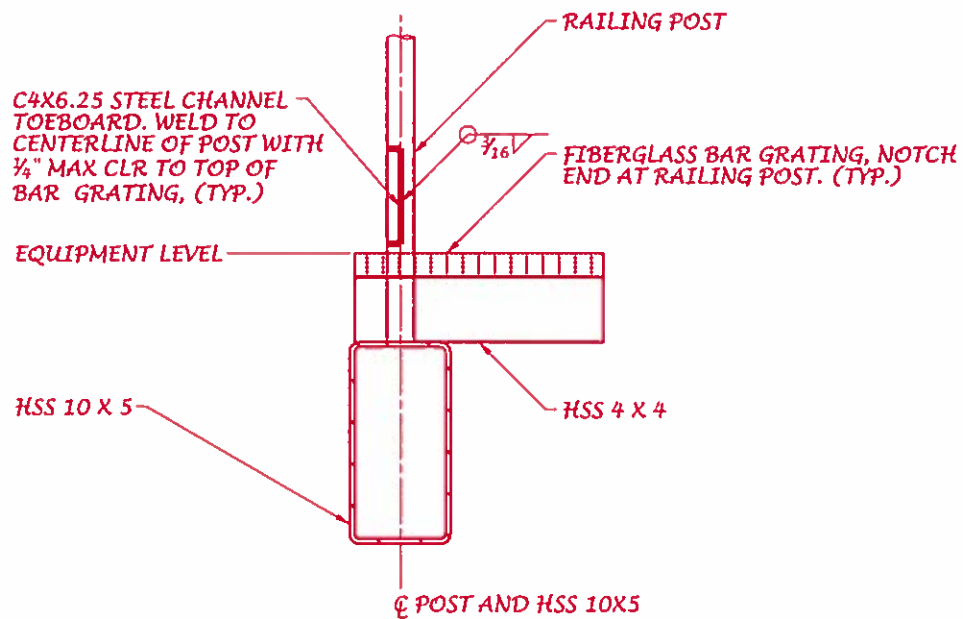
SHEET NO. BA617  
SHEET 1553 OF 1797 SHEETS

SR 520 FILE NO. 7051 SHEET BA616  
ProjectWise Vault

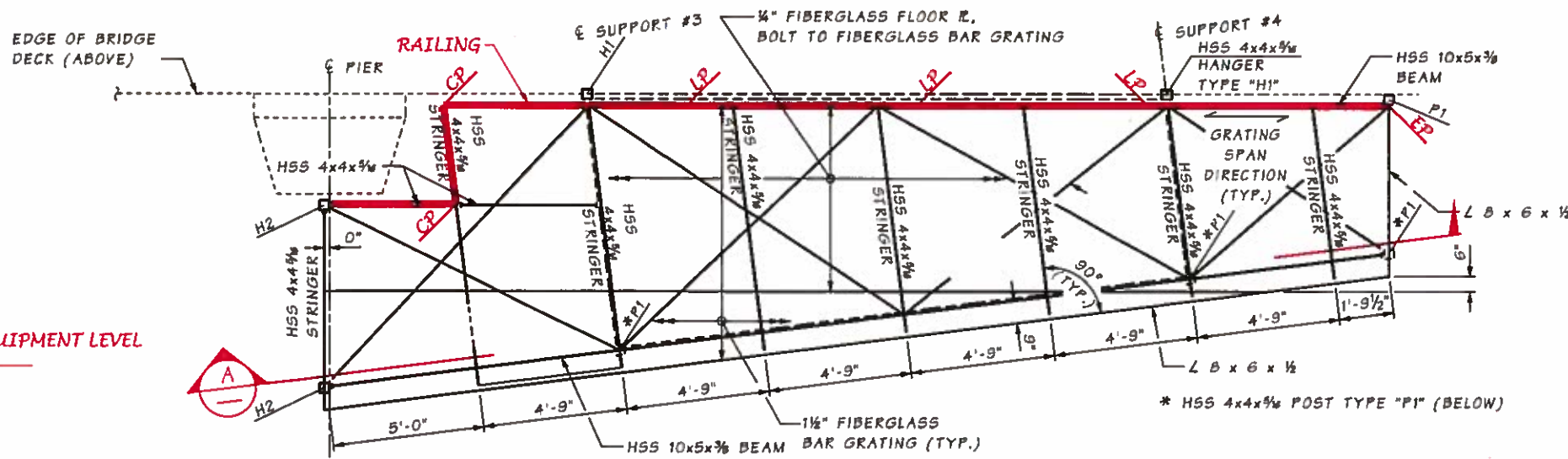


PROVIDE A 3/4" DIAM. HOLE AT PIPE CROSSINGS FOR GALVANIZING.

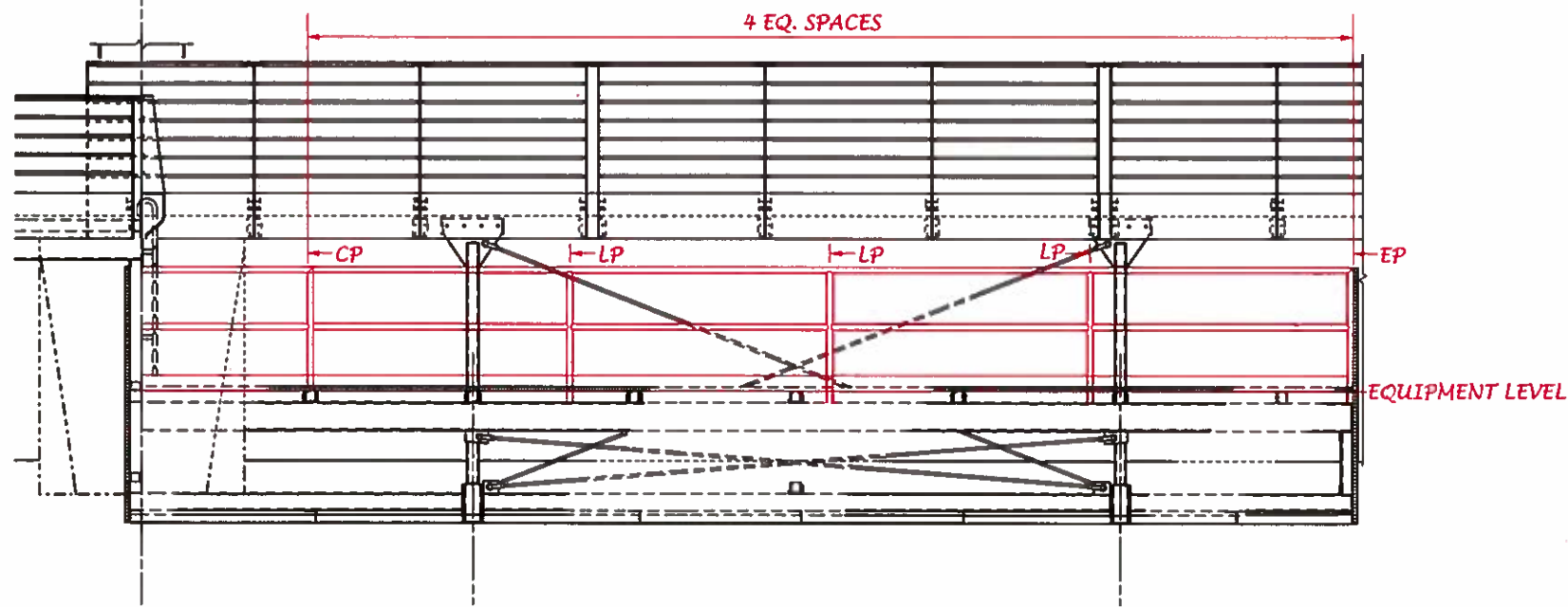
**RAILING - DETAILS**



**TOEBOARD - DETAIL**



**WEST HALF FRAMING PLAN AT EQUIPMENT PLATFORM BELVEDERE**  
EQUIPMENT LEVEL



**VIEW A**  
LP = LINE POST  
CP = CORNER POST  
EP = LINE POST

**EQUIPMENT PLATFORM BELVEDERE RAILING**

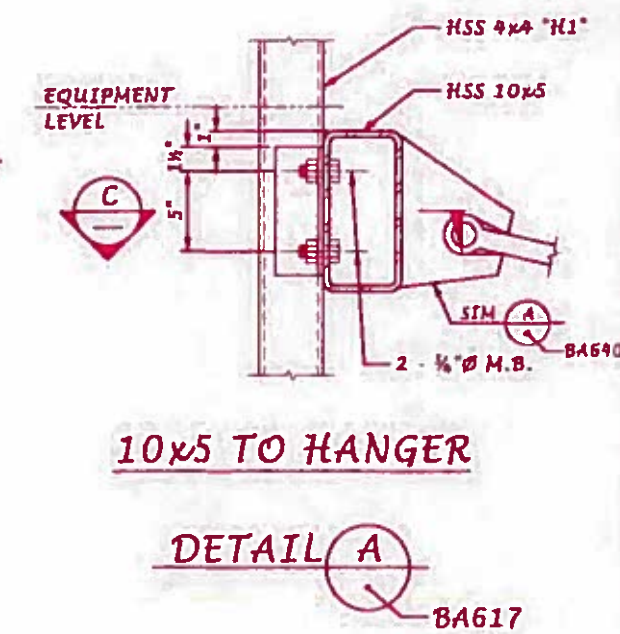
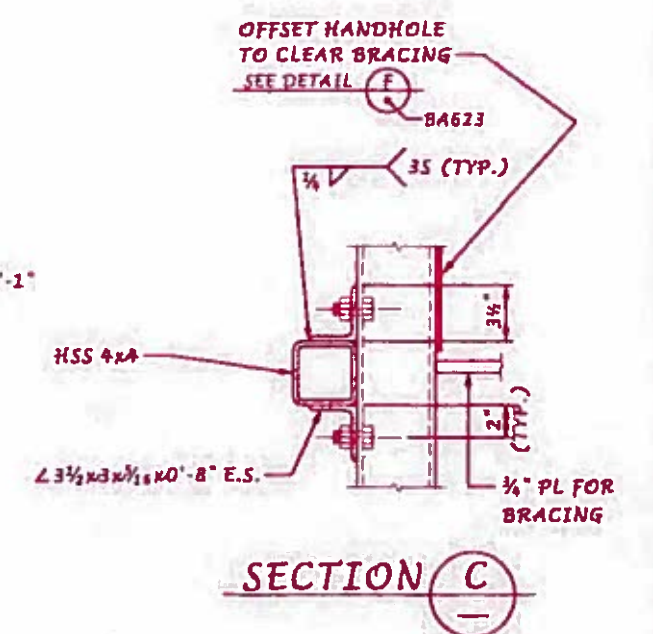
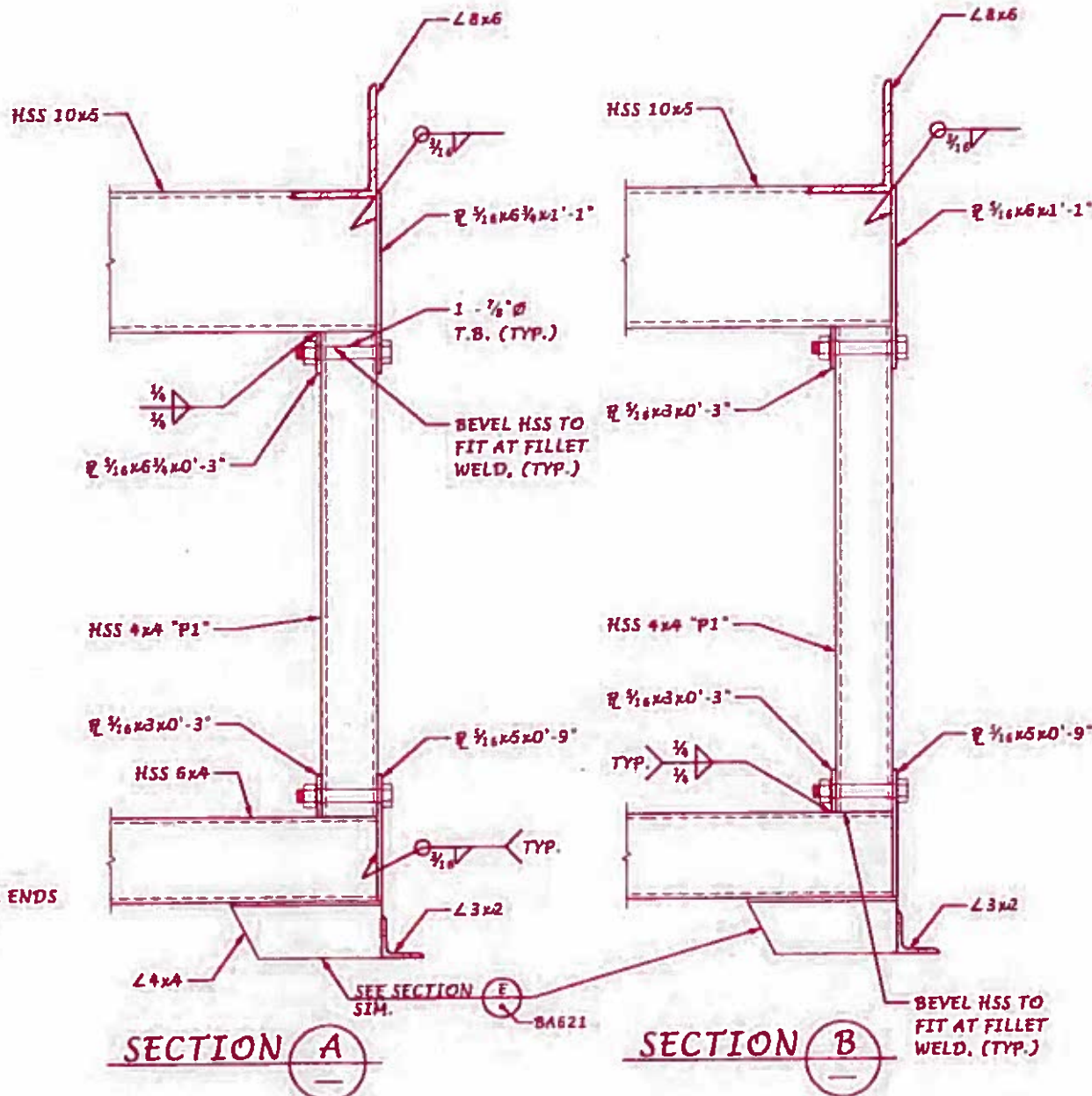
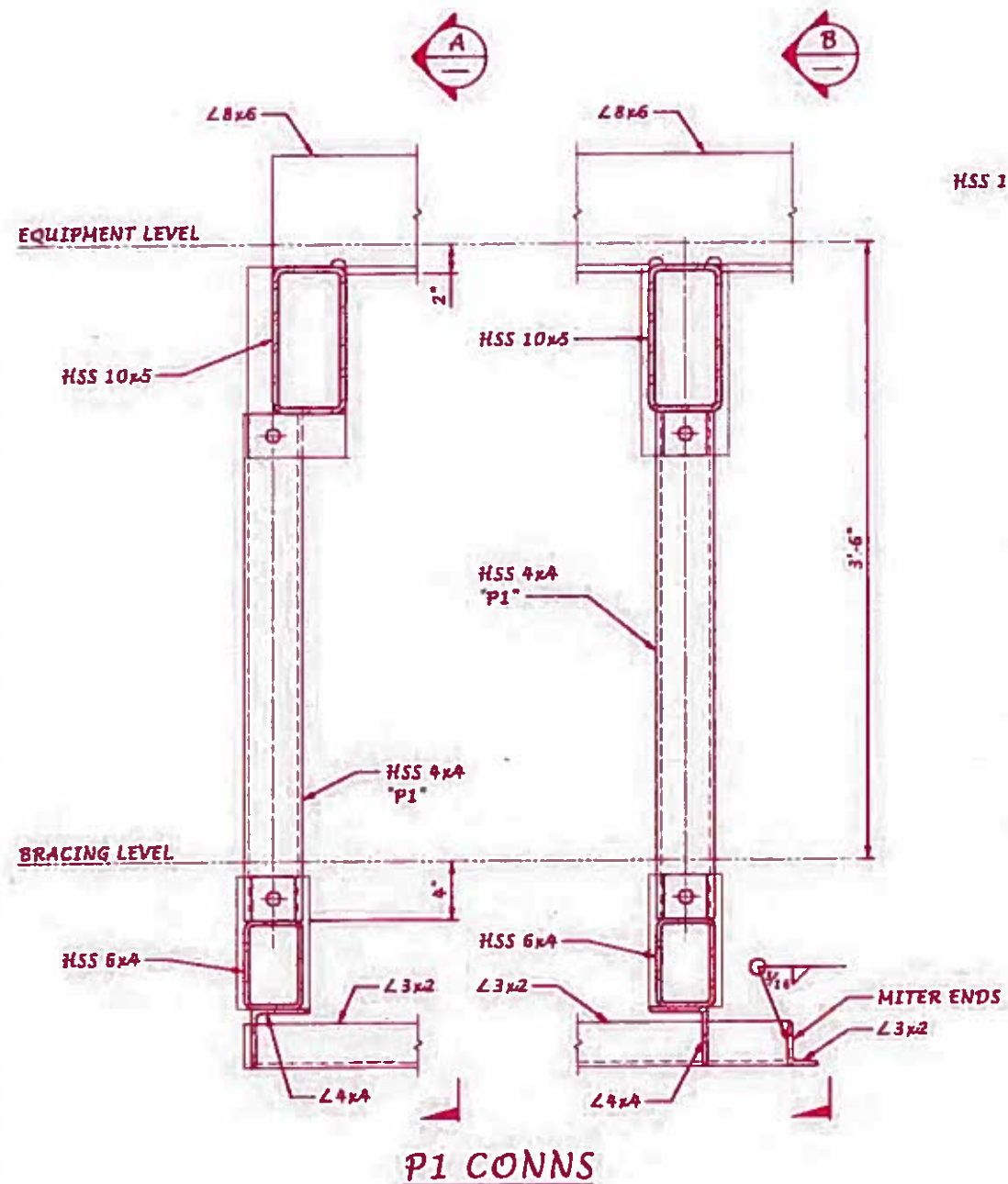
Bridge Design Engr.	PW:\Program\Design-Bld-Build\Contract_008625_WABN02.D.Contract Documents\2.05.Contract Plans\2.05.02_Aa-built\As-Built DGN's\1553.1_C8625_P8_BA_617.1.dgn				
Supervisor	BOTT, P.	REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO. TOTAL SHEETS
Designed By	KORPI, D.	10	WASH		1552 1797
Checked By	KNUTSON, G.	JOB NUMBER			
Detailed By	AUDAR, M.	13A012			
Bridge Projects Engr.		DATE	REVISION	BY	APP'D
Prelim. Plan By	10/21/15	CO#61 - EQUIP PLATFORM MOD	AS GGK		
Architect/Specialist					



Contract 8625  
Change Order #72  
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EQUIPMENT PLATFORM BELVEDERE  
FRAMING PLAN 2

BRIDGE SHEET NO. BA617.1  
SHEET 1553.1 OF 1797 SHEETS



Bridge Design Engr.	PW:\Program\Design-Bid-Build\Contract_008825_WASH02.0_Contract Documents\2.05_Contract Plans\2.05.02_Aa-bullsVa-Bull DGN\W011555.1_C0425_P2_BA_010.1_R1.dgn									
Supervisor	BOTT, P.	REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS				
Designed By	KORPI, D.	10	WASH		1555	1797				
Checked By	KNUTSON, G.	JOB NUMBER								
Detailled By	DEGUZMAN, A.	15A012								
Bridge Project Engr.										
Printn. Plan By	11/12/15	DATE	CD#61 - EQUIP PLATFORM MOD	DK	GGK	BY	APPD			
Architect/Specialist										



Contract 8625  
Change Order #72  
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**PARSONS BRINCKERHOFF** **Parametrix**

**BELVEDERE AND EQUIPMENT PLATFORM  
BELVEDERE FRAMING DETAILS 2**

BRIDGE SHEET NO. BA619.1  
SHEET 1555.1 OF 1797 SHEETS

### CHANGE ORDER-CHECKLIST

Cont. #: <u>8625</u> Cont. Title: <u>SR 520 West Approach Bridge North (WARN)</u> C.O. #: <u>72</u> C.O. Title: <u>Marine Casing Ext and Misc</u>	If yes, State Construction Office Approval Required.
<b>I. Executed by the State Construction Office</b>	
1. Cost or credit equal to or exceeding \$200,000.*1	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No      X
2. Change in the contract documents beyond the scope, intent or termini of the original contract.*2	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No      X
3. Any proposed revision or deletion of work that affects the condition of award requirements. (Must be coded "CO" in CCIS)	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No      X
4. Change in contract time greater than 30 working days, or a change in contract time not related to any change order.	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No      X
<b>II. Executed by the Region</b>	
5. Cost or credit greater than \$100,000 but less than \$200,000. *1	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
6. Change in contract time greater than 10 and less than or equal to 30 working days (must be related to changes implemented by change order).	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
<b>III. Executed by the Project Engineer</b>	
7. Determination of impacts and/or overhead.	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No      X
8. Specification change involving Headquarters generated specification. (Includes Region generated specification requiring State Construction Office Approval)	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No      X
9. Specification change involving Region generated specifications.	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
10. Material or product substitution.	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No      X
11. Structural design change in the roadway section. (Requires State Materials Lab approval)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
12. Determination of changed condition. (Section 1-04.7 of the Standard Specifications)	<input checked="" type="checkbox"/> Yes <input checked="" type="checkbox"/> No      X
13. Settlement of a claim. (Section 1-09.11(2) of the Standard Specifications)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No      X
14. Repair of damage regarding "acts of God" or "acts of the public enemy or of government authorities". (Section 1-07.13 of the Standard Specification)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No      X
15. Structural change to structures. (See BTA authority as shown in the Construction Manual)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No      X

**Approvals obtained:** Project Engineer (Required): Brian Dobbins Date: Multiple  
 Region (Required if yes marked): Dave Becher / Julie Meredith Date: Multiple  
 State Construction Office: Derek Case Date: Multiple  
 State Materials Lab: N/A Date: \_\_\_\_\_  
 Other (Local Agency, FHWA, Surety, etc.): Anthony Sarhan Date: 8/26/2016

**To be completed by the Project Engineer :**  
 CO Reason(s) (See "2008 Codes and Definitions" on HQ Construction SharePoint): AI-04 LIC AW DS  
 Change Order Prepared By: Brian Grieve Date: 10/23/2015  
 Has change been entered as lesson learned?  Yes  No      Has design documentation been updated?  Yes  No  
 Is this project under full FHWA stewardship oversight? \*1  Yes  No

**To be completed by the Region :**  
 Is the change eligible for Federal participation where applicable?  Yes  No  
 Change Order Reviewed by: Jon Danks Date: 12.18.16

\*1 Change (Cost or Credit) greater than \$200,000 or greater than 30 days on Full Federal Stewardship Oversight projects requires FHWA approval. (see Construction Manual - Chapter 1-2.4C(3), Chapter 1-3.4, and <http://www.wsdot.wa.gov/biz/construction/Stewardship/Stewardship.xls>)  
 \*2 Per RCW 47.28.050, any change beyond \$7,500 that is beyond the original scope shall go through the competitive bidding process.

This form represents the minimum information required by the State Construction Office. If you wish to supplement this information, you may do so on a separate sheet of paper.

**CHANGE ORDER-CHECKLIST**

**SR 520 Corridor Program: Change Order Page 2 - Checklist Supplement**

Cont. #:

C.O. #:

**To be completed by the Project Engineer:**

Does this change order require coordination with other SR 520 Projects / Program Areas?  Yes  No

Coordination has taken place with:

Contract Project (identify your project):

Pontoon	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A	Name: <input type="text" value="N/A"/>	Date: <input type="text"/>
Pontoon Mitigation	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A	Name: <input type="text" value="N/A"/>	Date: <input type="text"/>
Pontoon Moorage	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A	Name: <input type="text" value="N/A"/>	Date: <input type="text"/>
Eastside HOV	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A	Name: <input type="text" value="N/A"/>	Date: <input type="text"/>
Evans Creek	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A	Name: <input type="text" value="N/A"/>	Date: <input type="text"/>
FB&L	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A	Name: <input type="text" value="N/A"/>	Date: <input type="text"/>
WCB	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A	Name: <input type="text" value="N/A"/>	Date: <input type="text"/>
WABN	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A	Name: <input type="text" value="N/A"/>	Date: <input type="text"/>

Identified risk has been entered into the SR 520 Risk Management Database:  Yes  No  N/A

Risk ID Number (If pending, identify as Pending):

Project Risk ID No.:

Project Risk ID No.:

Project Risk ID No.:

**To be coordinated with Program Finance, Budget and Controls Group:**

Program Funding Concurrence?  Yes  No Funding Concurrence Obtained on: Date:

Is Sales Tax Included in CO Cost?  Yes  No Is sales tax deferred?  Yes  No

Max Payment Curve Changes discussed with:  Date:

Thrd Party Agreements discussed with:  Date:

**Coordination with Others:**

Design / Technical Lead: Name:  Date:

Maintenance: Name:  Date:

Environmental Commitments: Name:  Date: